

The background of the cover features several architectural sketches. On the left, there is a sketch of a building with a sign that says 'MARKET'. Below it is a sketch of a street scene with a wooden fence and a street lamp. On the right, there is a detailed street plan or site map showing building footprints, streets, and a winding path. The title 'Las Virgenes Gateway' is overlaid on these sketches in a large, elegant serif font.

Las Virgenes Gateway

MASTER PLAN

December 2, 1998

Prepared for the City of Calabasas
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Las Virgenes Gateway

MASTER PLAN

CITY OF CALABASAS
LAS VIRGENES GATEWAY MASTER PLAN

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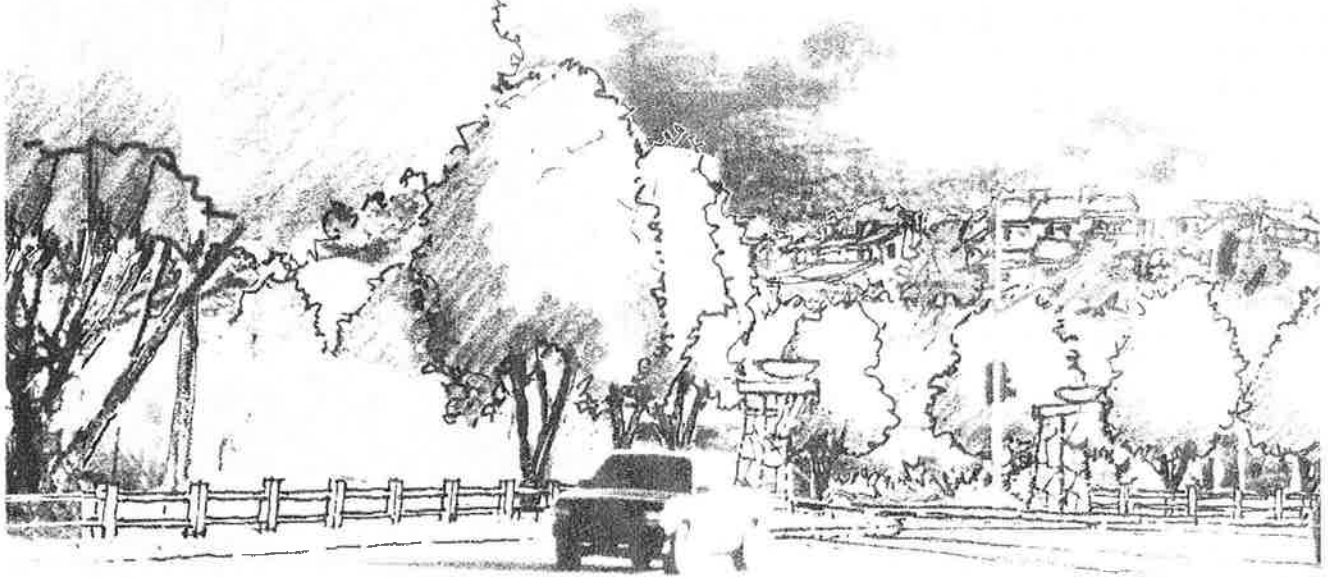
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CITY OF
CALABASAS

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Introduction

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INTRODUCTION

BACKGROUND

The Las Virgenes Road and Ventura Freeway interchange area has historically served as a rest stop and crossroads for travelers. This area is at the western edge of the San Fernando Valley and has been a gateway to the beaches along the Pacific Ocean and the beachfront cities. More recently, this corridor serves the neighborhoods in western Calabasas, as well as a route to Pepperdine University, Malibu Creek State Park and the Santa Monica Mountains.

Visually, the area contains some of the most scenic and diverse topography in the region. Calabasas has always been identified with the beauty of its natural environment: the rolling hills, oak woodlands, canyons, creeks and wildlife. While the rural character and natural beauty are still apparent, the encroachment of urban development has threatened to destroy much of this natural rural character.

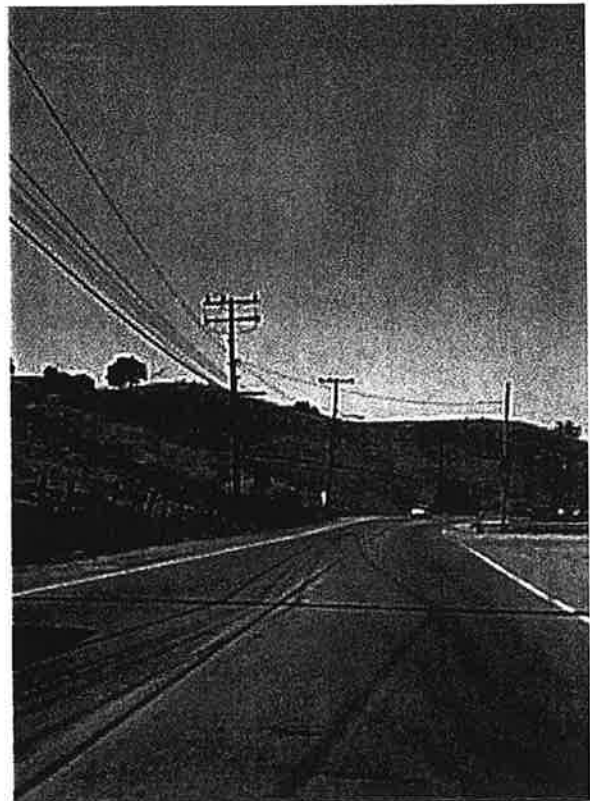
This threat occurs in the Las Virgenes interchange area that has seen a confluence of land uses, businesses and freeway-oriented roadway designs over the years. These uses developed first to serve the traveling public and secondarily the residents who moved into the adjacent neighborhoods. Much of this development occurred prior to the incorporation of the City of Calabasas and therefore, was done without the City's vision for its future. A lack of cohesive standards has led to visual clutter from too many driveways and commercial signs, a lack of landscaping and unaesthetic roadways and sidewalk areas.

This area could be jeopardized by pending development entitlements approved years ago by the County of Los Angeles. Some of this potential development appears to conflict with the General Plan vision for Calabasas as a rural, residential community. Such development could significantly alter the character of the area if allowed to develop independent of an overall vision for the area.

Some land owners in the gateway area recognize the need for planning and renovations to provide a safe and desirable area for residents and visitors. These land owners wish to develop the abandoned commercial sites and to make improvements to out-of-date commercial spaces. A cohesive plan to guide private property development is currently lacking and could provide a blue print for these renovations.

The City has always noted the value and the vulnerability of the Las Virgenes Road corridor. In the General Plan process, Las Virgenes

“Calabasas has always been identified with the beauty of its natural environment: the rolling hills, oak woodlands, canyons, creeks and wildlife. While the rural character and natural beauty are still apparent, the encroachment of urban development has threatened to destroy much of this natural rural character.”



“The General Plan’s vision for Calabasas is to maintain its traditional role as a refuge from the congestion of the San Fernando Valley and the metropolitan Los Angeles area.”

Road was identified as a “scenic corridor” and the City developed special regulatory measures to promote protection of its scenic qualities. After completion of the General Plan, the City Council recognized the need for additional planning for the Las Virgenes Road area and initiated the *Las Virgenes Road Corridor Design Plan*. This study was completed in January 1998 and addressed public improvements for roadways and sidewalks. That document identified the necessity for additional planning to address land uses and private property development/design standards. In April 1998, the City Council initiated this planning study to investigate the complex private land use and design issues that are encompassed within the gateway area.

This plan is intended to address the following issues:

- ♦ Provide proper urban design standards within the Plan boundary.
- ♦ Support neighborhood serving uses.
- ♦ Clarify land uses for the eastern hillside area.
- ♦ Regulate the mix of auto-oriented and neighborhood serving commercial land uses.
- ♦ Coordinate transportation planning in the corridor.
- ♦ Formalizing streetscape and public improvements in the Plan area.

PURPOSE OF THE MASTER PLAN

The General Plan’s vision for Calabasas is to maintain its traditional role as a refuge from the congestion of the San Fernando Valley and the metropolitan Los Angeles area. Thus, the General Plan’s Land Use Plan calls for a primarily low intensity residential community nestled in a natural setting. Urban development should not extend beyond the areas that are now developed or committed to urban uses. Rural residential uses should be located adjacent to the urban uses and should transition to open space uses. A key concept is that the rural, open character of lands be preserved. It is the City’s vision that any new development within the rural and open areas fit in with rather than replace the area’s natural environment.

For the Las Virgenes Road area, it is the General Plan’s intent that the natural hillsides dominate the freeway corridor at the Las Virgenes road interchange. The General Plan calls for development of the northwest quadrant to be limited to the lower portions of the hills so as to preserve oak woodlands and not dominate the views from the freeway. Freeway oriented commercial uses should continue in the southern quadrants of the Las Virgenes freeway interchange, focusing on urban design improvements to reduce visual clutter. New commercial development is called for along the east side of Las Virgenes Road at Agoura Road. A transition from commercial to

business park is called for along Agoura Road.

The dominant themes expressed in the City's General Plan are reinforced in the Las Virgenes Gateway Master Plan:

Environmental Responsibility - Preserving the area's remaining natural environment and living within the limits imposed by available resources. The Master Plan sets forth standards for preservation of open space, hillsides and creek areas. Also, traffic and circulation safety issues are addressed.

Local Management and Control - Accepting responsibility for managing Calabasas' affairs and its future in accordance with local values. The Master Plan establishes a vision and a clear set of rules by which development proposals will be reviewed.

Community Image - Protecting Calabasas' distinctive image. The Master Plan addresses the degradation that has occurred along this roadway through a proliferation of commercial signs, nondescript architecture and minimal landscaping. The Plan aims to enhance the natural beauty and improve the built environment along this scenic corridor.

The Las Virgenes Gateway Master Plan carries out the General Plan vision for this segment of the City while providing more specific land use and development criteria. The Master Plan recommends several General Plan land use designation amendments that are in keeping with the overall vision and the policies of the General Plan. After careful study of the land use issues in the Las Virgenes Road area, refinements to the original General Plan designations were developed, consistent with the vision and intent of the General Plan. In some cases, the Master Plan provides more specificity for new land uses and future development than was outlined in the General Plan.

As noted in the City's General Plan, "Calabasas was founded as a separate community, away from the urbanization and congestion of the Los Angeles metropolitan area." However, the City has inherited some elements of "urbanization and congestion" at its western gateway along the Las Virgenes Road intersection with the Ventura Freeway. The Las Virgenes Gateway Master Plan was formulated to reduce these negative attributes and provide a memorable gateway experience for local residents and visitors.

This Master Plan is intended to inspire and encourage renovation and appropriate new development opportunities. This Plan establishes a foundation upon which renovation and revitalization can occur within the rural context of Calabasas. Concepts are shown for streetscape refurbishment, facade renovations and new develop-

"The Las Virgenes Gateway Master Plan carries out the General Plan vision for this segment of the City while providing more specific land use and development criteria. The Master Plan recommends several General Plan land use designation amendments that are in keeping with the overall vision and the policies of the General Plan."



ment at key sites. The Master Plan enhancement strategies are balanced with development standards and restrictions that will provide land use compatibility and environmental protection.

This document augments the City's General Plan goals and policies and the Development Code standards. In some cases, General Plan and zoning designation changes are recommended by the Master Plan due to changes in development circumstances since the General Plan was adopted. These General Plan Amendments contained in the Master Plan will be concurrently adopted upon adoption of the Master Plan. Development standards presented in this Master Plan may be more restrictive than those required by the Development Code. In all cases, the standards set forth in this document will take precedence over those in the Development Code for properties in the Master Plan area.

This Plan plays an important role in the future of the Las Virgenes Gateway area by providing the following benefits:

- * A community vision for the area using realistic development scenarios;
- * A long-range blue print for appropriate change, eliminating uncertainty for decision-makers and property owners;
- * A tool for promoting revitalization of the existing commercial properties. The plan can be a marketing tool to attract desired development such as a neighborhood shopping center;
- * A tool in obtaining grants for public improvements, streetscape improvements, trail construction and creek restoration; integration of guidelines for public and private property architectural and landscape design.



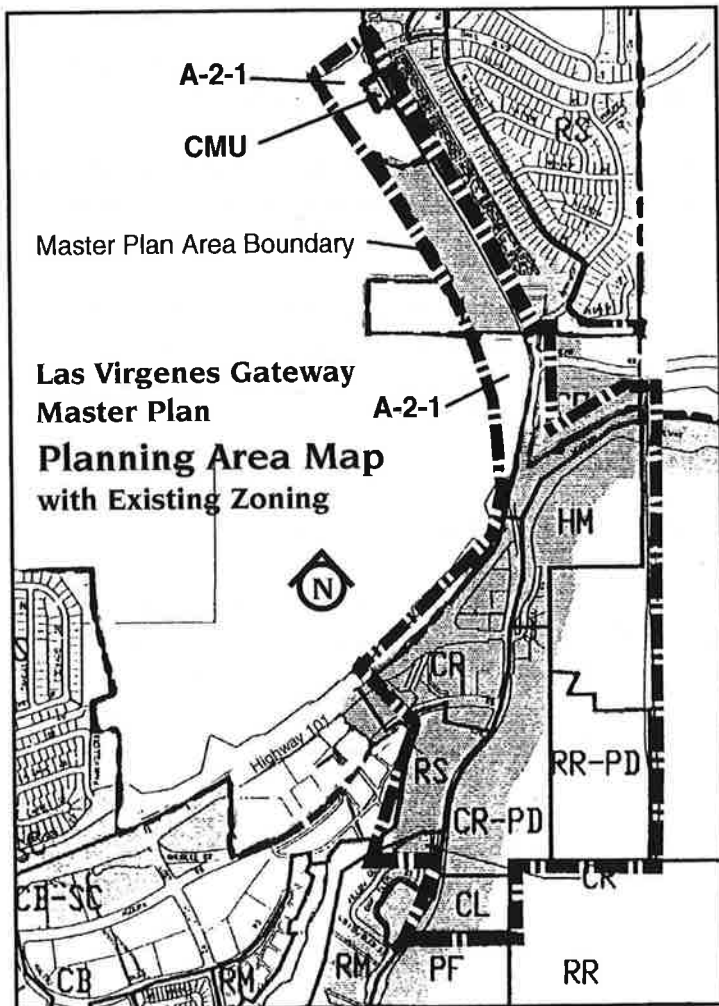
THE PLANNING AREA

The City of Calabasas is located in western Los Angeles County, at the edge of the San Fernando Valley. The Ventura Freeway (Highway 101) runs east/west through the middle of the City. Neighboring cities include Agoura Hills, Hidden Hills and Los Angeles. A portion of the City's western boundary abuts the Los Angeles County border and a portion of the northern boundary abuts Ventura County.

The Las Virgenes Gateway Master Plan encompasses the lands immediately adjacent to the Las Virgenes Road corridor. The area extends along Las Virgenes Road from the Water District Headquarters on the south side to Thousand Oaks Blvd on the north side. The eastern boundary is approximately along the hillside ridge top

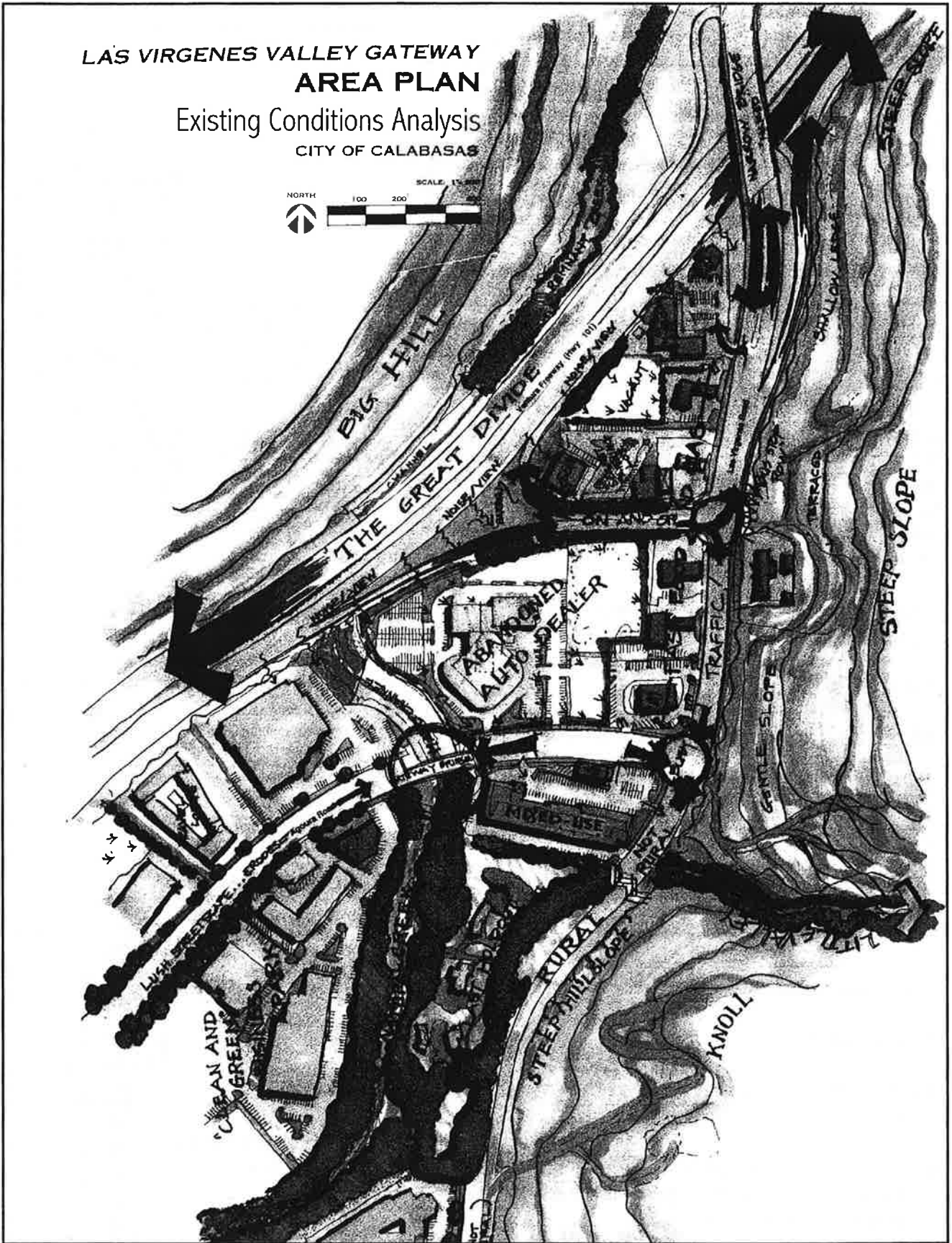
for the area south of the freeway. To the north of the freeway, the eastern boundary runs along Las Virgenes Road. The west side boundary is at Malibu Creek in the area south of the freeway and the top of the western ridgeline for the lands north of the freeway. Some of the lands north of the freeway are not within the City boundaries. While the City has no decision-making authority for development proposals in either Ventura or Los Angeles County, these lands are an integral part of the community and should reflect the quality and character of the City's vision. As the City intends to influence the design and development of these lands, during development review for County projects, the City will recommend that the standards contained in this Plan be adhered to.

The planning area encompasses the Ventura Freeway (Highway 101) on and off ramps and a small commercial area with highway and auto-oriented uses including gas stations, fast food and a sit-down restaurant. A large vacant commercial parcel and several



LAS VIRGENES VALLEY GATEWAY AREA PLAN

Existing Conditions Analysis
CITY OF CALABASAS



vacant gas station sites are located adjacent to the highway oriented businesses. To the east of Las Virgenes Road and the commercial area are undeveloped hillsides that have been the subject of various development proposals over the years. To the north of the freeway, lands are developed with multi-family housing on the east side of Las Virgenes Road. Large tracts of undeveloped hillsides rise above Las Virgenes to the west. A small neighborhood commercial center is located just south of Thousand Oaks Blvd.

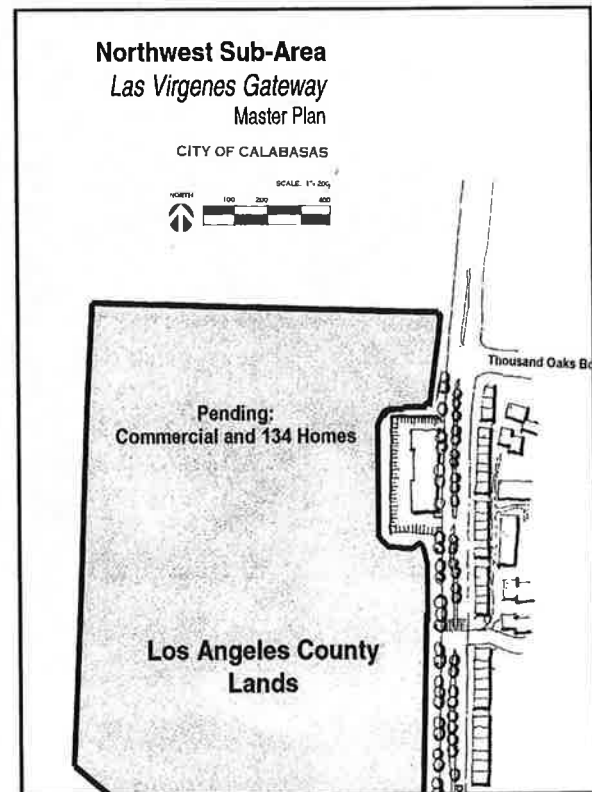
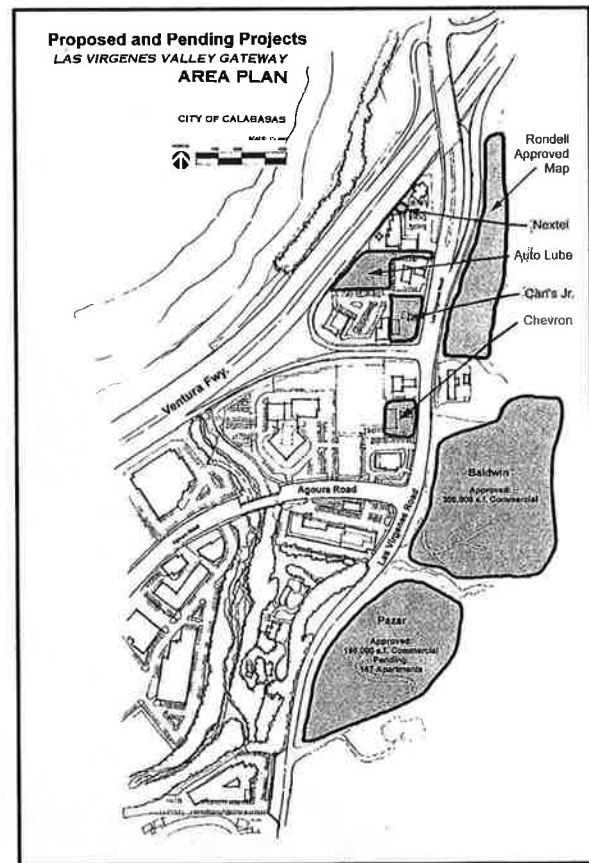
OPPORTUNITIES AND CONSTRAINTS

The Las Virgenes corridor has been shaped by both natural and man made forces. Geography has a strong influence on the form of the corridor. High hills rise above the roadway with steep green valleys and prominent ridgelines. This provides a striking backdrop for the man made elements that line the roadway. At the same time, the hillsides present challenges for development including geologic instability, erosion, access and view preservation. The Malibu Creek traverses along the western edge of the study area forming a natural boundary for the Plan. A portion of this creek has been channelized with concrete thereby preventing the development of creekside habitat and recreational amenities.

The opportunities apparent in the Master Plan area include the following attributes:

- ♦ Large undeveloped parcels of land.
- ♦ Hillsides that provide a majestic and rural backdrop.
- ♦ The contrast between the intensity of urban development and the openness and scenic beauty of the surrounding hillsides, canyon and creek areas.
- ♦ The Las Virgenes Creek corridor.
- ♦ The vacant seven acre auto dealership parcel on Agoura Road.
- ♦ The compact development of existing businesses.
- ♦ Freeway visibility.
- ♦ Land owners that are willing to work together to enhance the area.

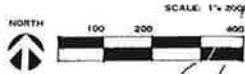
The following list of issues represent concerns or constraints expressed at the Community workshops and public hearings, as well as issues identified by the Project Team:



Existing Opportunities for Change

LAS VIRGENES VALLEY GATEWAY AREA PLAN

CITY OF CALABASAS



Gateways

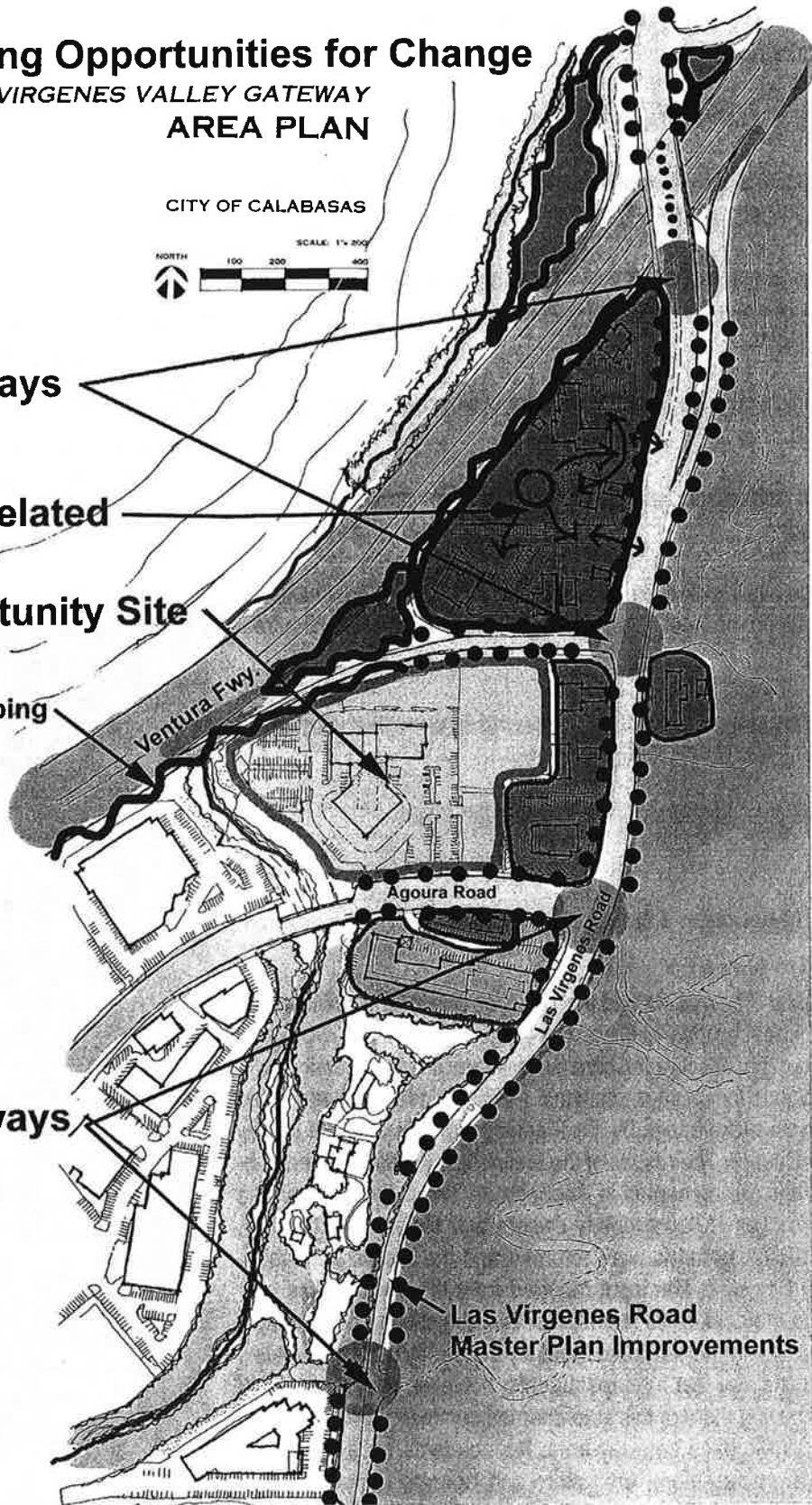
Auto-related
Uses

Opportunity Site

Freeway
Landscaping

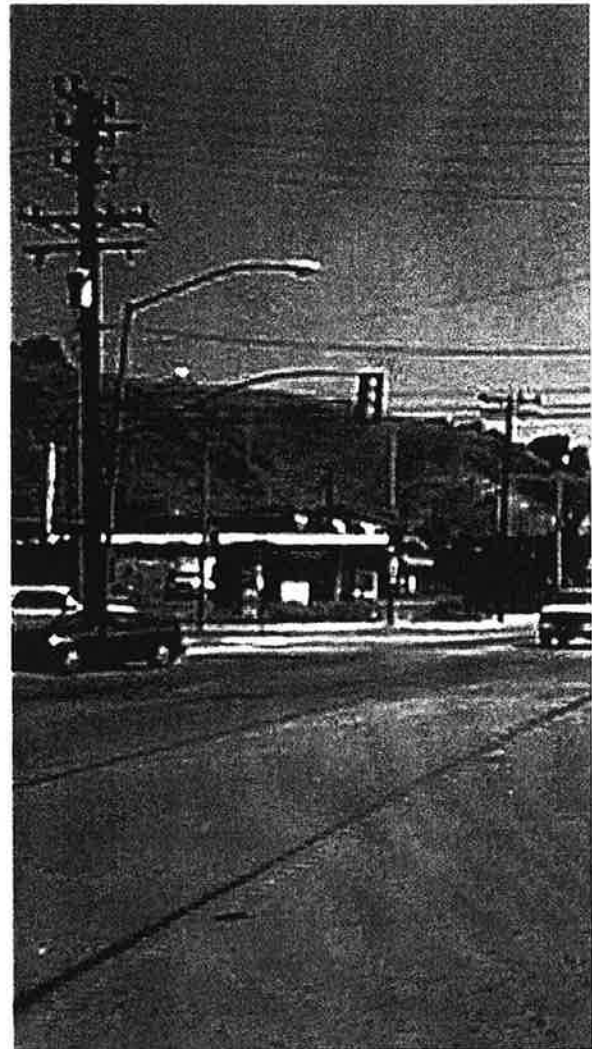
Gateways

Las Virgenes Road
Master Plan Improvements



- ♦ Visual clutter along Las Virgenes Road between the freeway and Agoura Road.
- ♦ Lack of a cohesive design statement for private property.
- ♦ Steep, unstable slopes where topography acts as a limitation on various land uses and types of development.
- ♦ Vehicle safety issues due to poorly coordinated ingress and egress along Las Virgenes Road between the freeway and Agoura Road.
- ♦ Poorly coordinated parking areas for business along Las Virgenes Road.
- ♦ Prior entitlements for large commercial developments on the east hillsides.
- ♦ Large parcels of land that are under Los Angeles County jurisdiction along the west side of Las Virgenes Road, north of the freeway.
- ♦ Current applications for development that seem inappropriate or improperly designed for the area.
- ♦ Excessive numbers of auto-oriented land uses.

Survey respondents indicated that they frequently used the existing gas stations and fast food restaurants, however, they were not in favor of having more of this auto-oriented commercial development in the Las Virgenes gateway area.



PLANNING ISSUES AND COMMUNITY INPUT

This Master Plan was created from an open public forum using many public outreach opportunities. The Project Team held two public workshops that encouraged community interactions and "brainstorming" sessions for creative solutions. Four public hearings were held by decision makers; two by the Planning Commission and two by the City Council. The results of the preliminary workshops and public hearings are presented in Appendix A. The City also mailed a survey to over 11,000 property owners and residents in the City. Over 630 survey forms were returned and the results were tabulated by City staff. The input received from the surveys was very similar to the comments received at the workshops and hearings. Survey respondents indicated that they frequently used the existing gas stations and fast food restaurants, however, they were not in favor of having more of this auto-oriented commercial development in the Las Virgenes gateway area. Respondents were in favor of adding a grocery store, drug store and upscale restaurant. The majority of survey responses indicated a desire to protect the east

The majority of survey responses indicated a desire to protect the east hillsides from intense development.



hillsides from intense development. The preferred hillside land uses were open space, public park, and grazing. If new development were to occur, there was a preference that it be restricted to one story heights and a design that blends into the hillside environment. A summary of the survey results is presented in Appendix B.

The major land use and design issues raised during the community workshop, public hearings and through mail-in surveys are as follows:

- ♦ Auto/highway related uses should be limited to the area adjacent to the freeway.
- ♦ A master internal parking/circulation plan is needed for the commercial areas.
- ♦ A common architectural theme should be developed that emphasizes rural/rustic styles.
- ♦ A common landscaping theme should be developed that is coordinated with the public improvements specified in the Las Virgenes Corridor Plan.
- ♦ Fast food uses are okay but should be limited to the area adjacent to the highway and these uses should not involve drive-through service.
- ♦ The visual clutter along Las Virgenes Road from the freeway to Agoura Road should be reduced.
- ♦ Signage needs to be centralized and consistent with the design theme.
- ♦ The views of the hillsides need to be preserved.
- ♦ Driveways should be provided to the Land Conservancy open space lands.
- ♦ Driveway access should be limited off of Las Virgenes Road.
- ♦ Open space should be retained on the hillsides.
- ♦ Environmental issues need to be addressed for any development on the hillsides.
- ♦ Development on the East Hillsides should be low profile residential uses.
- ♦ New development off Agoura Road should orient to the creek.

- ♦ The natural creek banks and habitat should be restored.
- ♦ Trails are desirable along the creek and to connect the residential areas to the commercial area.
- ♦ Local residents need neighborhood serving uses such as a grocery store, library, coffee shop, bookstore and pharmacy. This will reduce the need for residents to travel out of the area to shop.
- ♦ Streets and intersections need to be pedestrian friendly.
- ♦ Thousand Oaks Blvd should not be extended to the west.
- ♦ The County lands along the Las Virgenes corridor should be considered for annexation. Possible uses include low density residential, a public school and recreation fields.
- ♦ The Northside neighborhood commercial area should be remain.
- ♦ Overhead utility lines should be removed.
- ♦ Senior housing is needed in the area, especially a "continuum of care" type complex.
- ♦ Telecommunication antennas are needed in the area however they need to be sensitively designed and located.

The Master Plan encompasses the issues raised in the planning process. Existing land use incompatibilities and proposed projects that may require Zoning or General Plan changes are addressed through a revised land use plan for the area. A scheme for coordinated internal circulation and access issues is presented. The Plan also sets forth an appropriate architectural and landscaping theme for private property in this area of the City.

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2 The Vision and Theme

THE VISION AND THEME

THE LAS VIRGENES GATEWAY: COMMUNITY AND NATURE IN BALANCE

Narrative Vision Statement

The Las Virgenes Road corridor is a memorable district. Handsome and rustic, its stone monuments welcome residents and visitors to this area as the gateway to western Calabasas, the Santa Monica Mountains and Malibu Creek State Park.

The rural setting is punctuated by lush indigenous landscaping that lines the roadway and center road median. The architecture of new and remodeled buildings reflects the rural character of traditional Southern California. Sidewalks and driveways are lined with rail fences set into stone pillars. There are no overhead utility lines.

Highway-related services and a park n' ride lot adjacent to the freeway are easily accessible to residents and visitors. On the east side of the Las Virgenes corridor, green hills tower over clustered residential development that is tucked into the slope and well screened by abundant landscaping. Below the homes, a compact office complex provides employment opportunities and a Malibu Creek/Santa Monica Mountains visitor center.

To the west of the corridor, the Agoura Road Village shopping center offers a variety of shops and services that cater to local residents' daily needs as well as a branch library. Patrons of the center arrive by car, foot and bicycle to enjoy the outdoor patios, terraces and creekside paths. The newly reclaimed creek is lush with willows and cattails that provide habitat for a myriad of bird species.

Along the creek, to the south of the neighborhood commercial center, a senior residential complex provides living accommodations including apartments, assisted living and a nursing care facility. A meandering path along the creek provides a connection to adjacent neighborhoods and the commercial center.

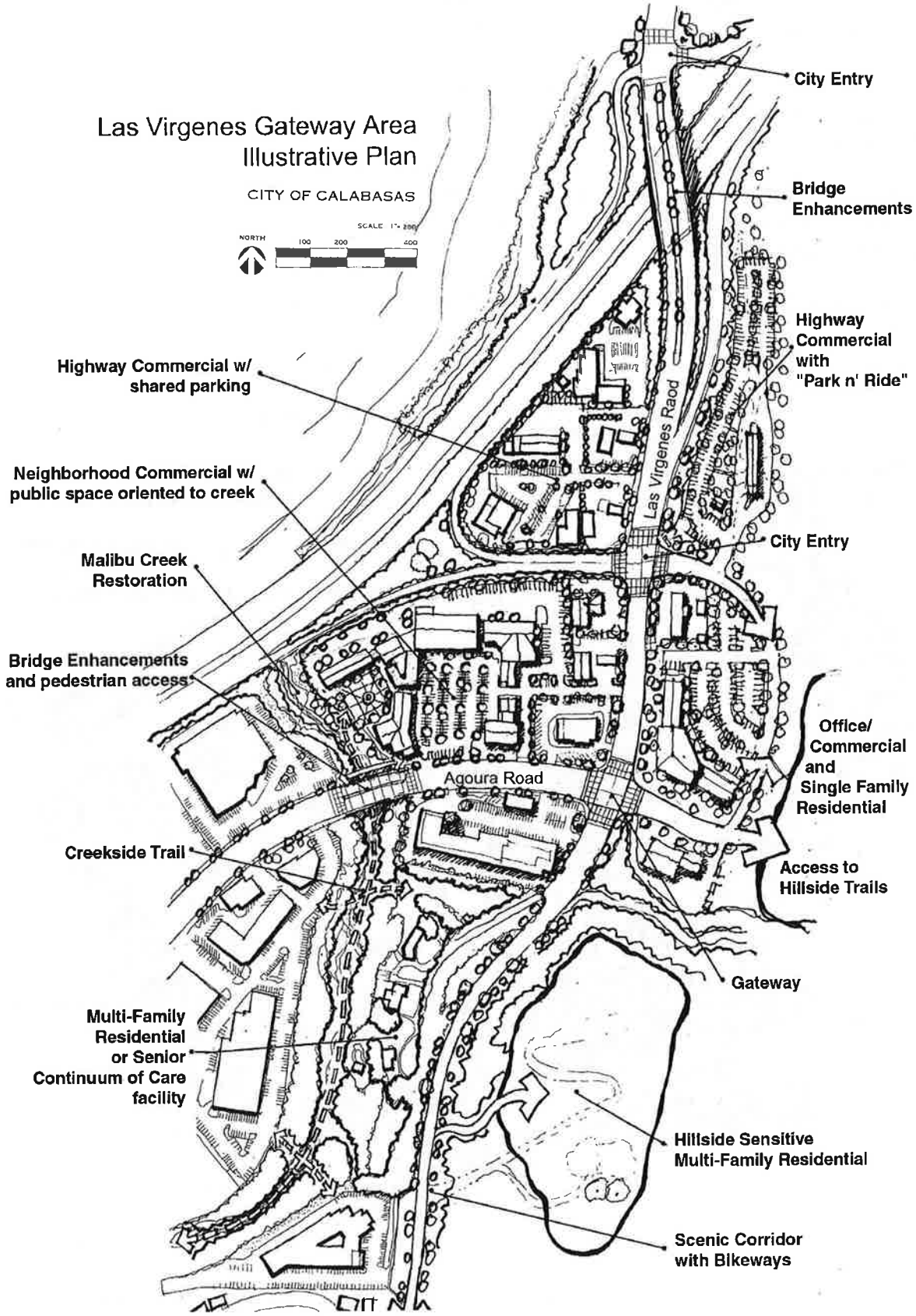
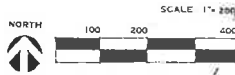
This narrative vignette establishes the guiding vision for the future of the Las Virgenes Gateway Area presented in this Master Plan. This vision includes the following components:

- ◆ A small thriving neighborhood commercial center that serves local residents with shops and services that meet everyday needs.

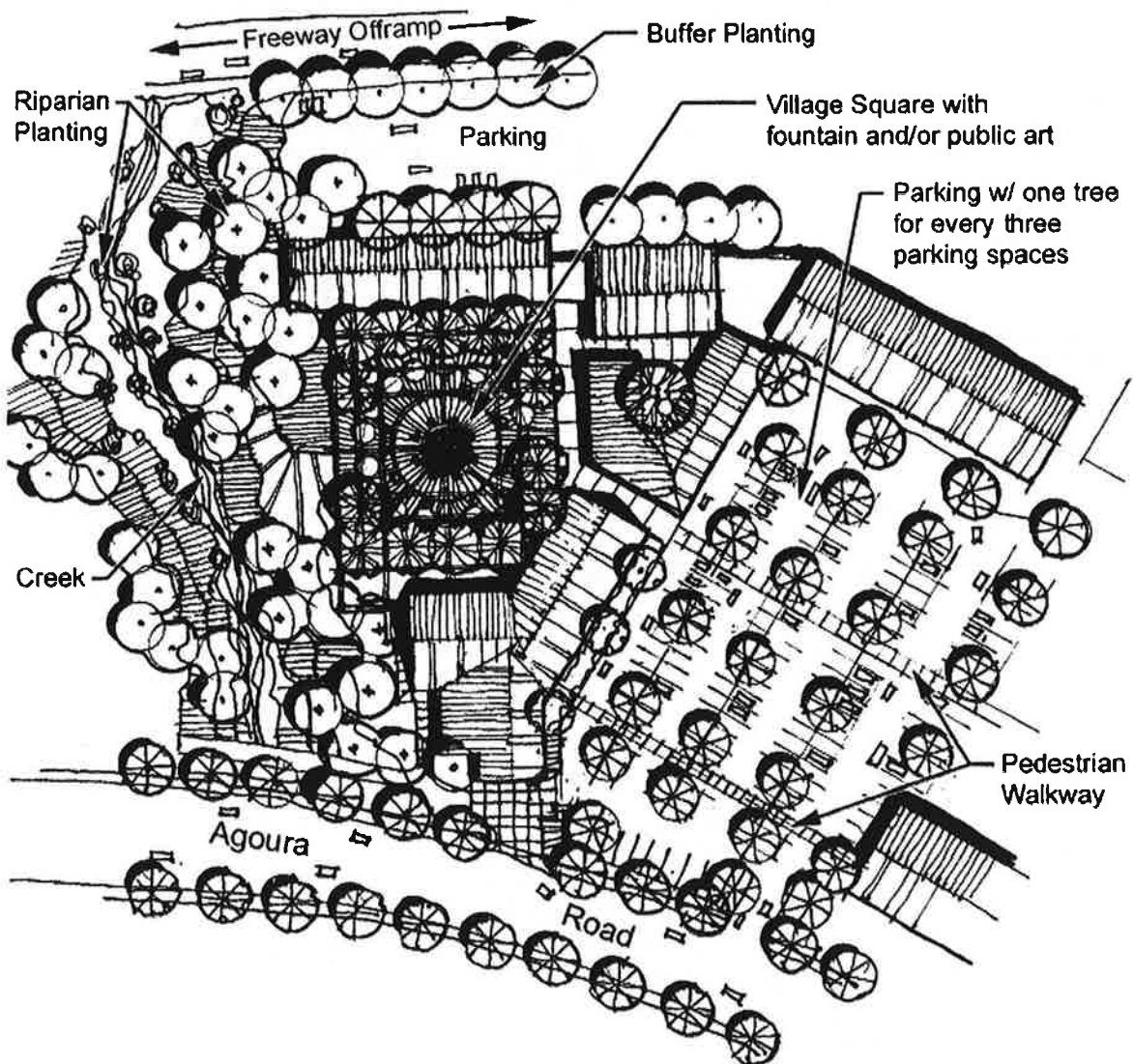
“The rural setting is punctuated by lush indigenous landscaping that lines the roadway and center median. The architecture of new and remodeled buildings reflects the rural character of traditional Southern California. Sidewalks and driveways are lined with rail fences set into stone pillars.”

Las Virgenes Gateway Area Illustrative Plan

CITY OF CALABASAS



- ♦ A highway oriented commercial center that provides auto services and quick food for visitors, as well as local residents.
- ♦ A modest yet cohesive streetscape that draws visitors from the highway along the Las Virgenes corridor to Malibu Creek Park and the Santa Monica Mountains.
- ♦ Monumentation at Highway 101 is prominently displayed to establish this area as a gateway to western Calabasas as well as the state and national parks.
- ♦ Traffic flow and parking is managed to provide safe vehicular



New commercial retail around an open village square are envisioned for the abandoned auto dealership. A reclaimed creek with restored planting will grace the western edge of the development .

movements and access for pedestrians.

- ♦ Pedestrian access from the business park to the west is enhanced to draw lunchtime walkers to the neighborhood commercial center.
- ♦ A reclaimed creek that collects drainage for the area, provides control of flood waters as well as plant and animal habitat. This natural environment also provides a peaceful respite from urban experience.
- ♦ A multi-use trail along the creek corridor connects the southern residential areas to the neighborhood commercial center and encourages local residents to walk or bike to their destinations.
- ♦ Low profile residential development is tucked into the east hillsides.
- ♦ A small office/commercial center located at the base of the east hillsides along Las Virgenes Road at the Agoura Road intersection.
- ♦ A linear park through the area links Malibu Creek State Park to areas north of the freeway.

An illustrative graphic of the Las Virgenes Gateway Master Plan is provided here. This graphic is a conceptual overview of the land use and urban design strategies for the area. The focus of the plan is to recommend appropriate land uses, suggest internal circulation and parking improvements and encourage streetscape improvements. The Plan adoption process will also implement the necessary General Plan Amendments. The illustrative plan shown here is not intended to specify actual projects to be built or actual site plan layouts. It also does not bind the City or property owners to building specific projects. However, the site plan recommendations should be considered in the review and approval of project applications.



3 Master Plan Goals

MASTER PLAN GOALS AND OBJECTIVES

The Master Plan establishes a foundation for appropriate growth within economic and environmental parameters. The Plan seeks to assure that new development and change to existing developments occur in a manner that is compatible with the community's vision.

This section of the plan sets goals and objectives to guide land use planning efforts, revitalization plans and for the review of new private development proposals. These statements are guidelines for decision making and indicate direction, priorities and the vision for the future.

The Master Plan has four key goals and various objectives that provide the policy framework for the Plan. The Goals are general statements that promote the vision. Objectives provide specific direction for accomplishing the goal. These objectives were defined in the community workshops and have been refined by the City decision makers in their public hearings:

“ The Plan seeks to assure that new development and change to existing developments occur in a manner that is compatible with the community’s vision.”

GOAL 1 - ENHANCE THE AESTHETICS OF THE LAS VIRGENES GATEWAY AREA AND PROMOTE THE COMMUNITY’S RURAL CHARACTER.

Objective 1.1 - Provide a unified rural vision and theme for the architecture of private development and for private property landscaping.

Objective 1.2 - Provide design standards for private property to carry out and enforce the community's rural vision and theme.

Objective 1.3 - Integrate the design elements with the Las Virgenes Corridor Plan, the General Plan and the Scenic Corridor Ordinance.

Objective 1.4 - Provide a plan for “gateway” Monumentation.

Objective 1.5 - Provide sign standards consistent with the rural theme to control signage and create an aesthetic gateway.

Objective 1.6 - Provide for the removal of existing non-conforming freeway-oriented pole signs.

Objective 1.7 - Require a component of new development on the former auto dealership property to orient to the creek. Wood decking with seating, a plaza area and pedestrian paths should be included in the development plans.

GOAL 2 - PRESERVE THE ENVIRONMENTAL INTEGRITY OF NATURAL FEATURES AND PREVENT SIGNIFICANT ENVIRONMENTAL IMPACTS.

Objective 2.1 - Provide special development standards to protect and enhance natural features including the hillsides and Malibu Creek.

Objective 2.2 - Integrate development standards of the General Plan, the Development Code and the Scenic Corridor Ordinance into Master Plan standards.

Objective 2.3 - Tailor established development standards for protection of hillside view corridors to meet Las Virgenes Gateway needs.

Objective 2.4 - Provide a plan for safe and efficient vehicle access and parking.

Objective 2.5 - Provide a plan for enhanced pedestrian access.

Objective 2.6 - provide a plan for restoring Malibu Creek to a more natural form.

GOAL 3 - PROVIDE A LAND USE PLAN THAT MAINTAINS A BALANCE OF USES, COMPATIBLE WITH THE EXISTING SURROUNDING NEIGHBORHOODS.

Objective 3.1 - Address the appropriate land use for parcels with approved or pending projects that have conflicts with the General Plan, Zoning or adjacent land uses.

Objective 3.2 - Establish specialized land uses designations and development standards to address hillside lands.

Objective 3.3 - Establish specialized land uses designations and development standards for a neighborhood serving village center.

Objective 3.4 - Establish specialized land uses designations and development standards to address highway-oriented land uses.

Objective 3.5 - If the neighborhood serving commercial center has not developed within five years of Plan adoption, the City should perform an economic/marketing study for the area. The findings of this study should be considered for determining if modifications to the land use plan and development standards are appropriate.

GOAL 4 - PROVIDE AN IMPLEMENTATION PLAN TO CARRY OUT THE LAND USE PLAN, THE DESIGN STANDARDS AND THE PUBLIC IMPROVEMENTS.

Objective 4.1 - Implement the General Plan Amendments and Zoning changes to carry out the Master Plan.

Objective 4.2 - Develop the Master Plan as a marketing tool to encourage appropriate new development such as a neighborhood serving commercial center.

Objective 4.3 - Develop a creek restoration plan that can be used to secure grants for creek enhancements or other environmental and/or recreational funding opportunities.

Objective 4.4 - Develop a plan for streetscape improvements consistent with the Las Virgenes Corridor Plan.

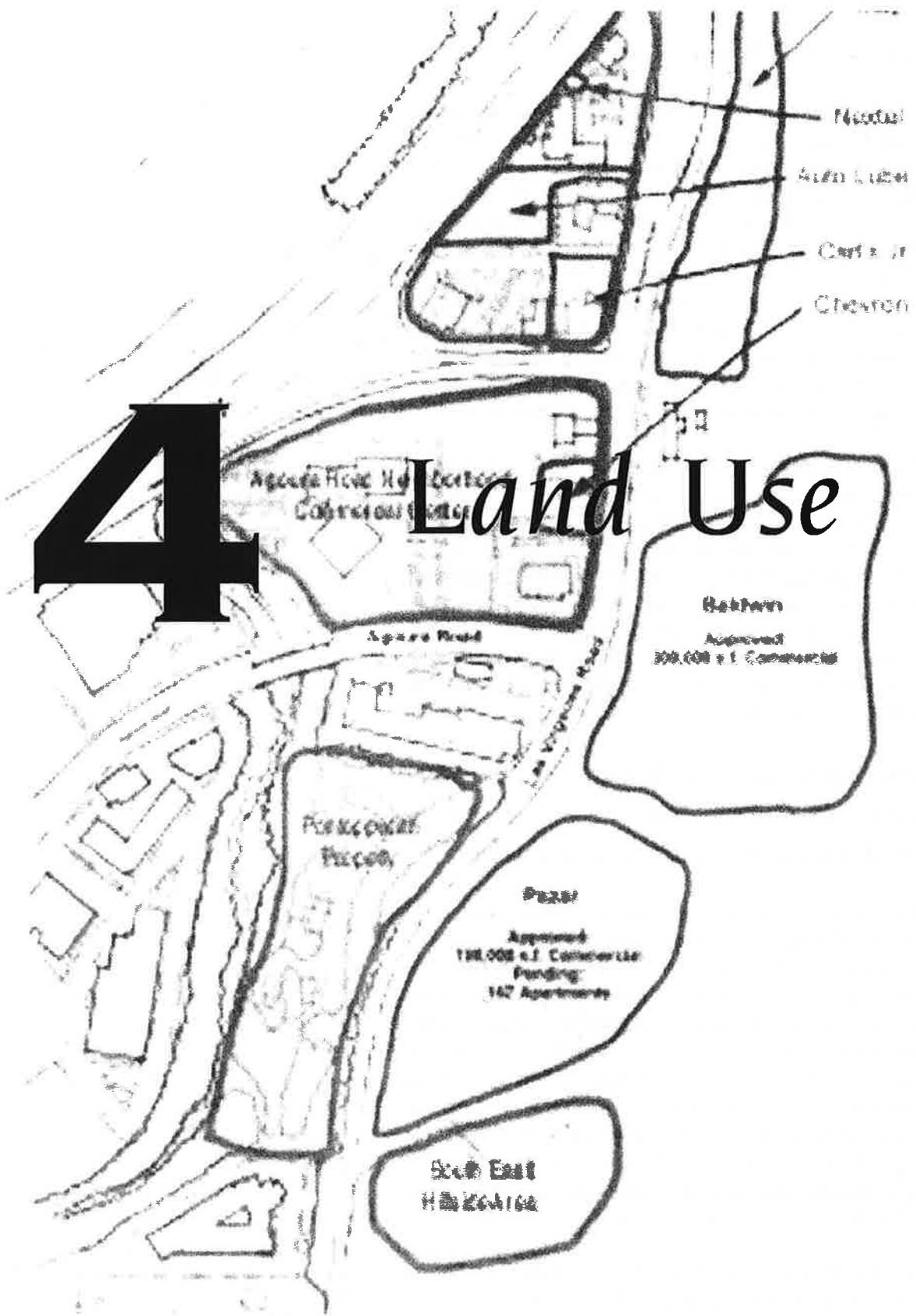
Objective 4.5 - Address methods to provide landscaping along freeway edges.

Objective 4.6 - Develop and adopt architectural and landscape design standards for use in the development review process.

Objective 4.7 - Develop and adopt sign standards for use in the development review process



4 Land Use



LAND USE PLAN

The City of Calabasas has a distinctive character derived from the oak studded hillsides, the green open spaces and the small pockets of commercial and residential development. The Las Virgenes corridor that serves as the western gateway to the City should echo this distinctive character. A significant concern in the Las Virgenes Gateway area is the type, intensity, location and character of land uses that will be permitted in the future. The Land Use Plan contained in this Master Plan addresses permitted, non permitted and encouraged land uses within the planning area. The location of each land use is indicated on the Land Use Map exhibit. To implement the Land Use Plan, General Plan and Development Code amendments will be required. These amendments are outlined in Chapter 9.

“A significant concern in the Las Virgenes Gateway area is the type, intensity, location and character of land uses that will be permitted in the future.”

It should be noted that several approved but not built developments on the east hillsides predate adoption of the City's General Plan and this Master Plan. The City acknowledges its legal responsibility to recognize valid development agreements and permits. However, the City also recognizes that given current market demands and project status, such development may not occur. In formulating the Master Plan, the City defined land uses and development intensities that reflect a compromise that is consistent with the Master Plan goals. The Plan recommendations are the preferred alternatives for new development in these areas.

EXISTING LAND USE DESIGNATIONS, GENERAL PLAN POLICIES AND DEVELOPMENT STANDARDS

The City has adopted strong measures to promote appropriate development within the City. These measures include General Plan polices, the General Plan Performance Standards, the Scenic Corridor Ordinance and Development Code Standards. All of these measures will continue to provide the criteria upon which new development will be reviewed. The Land Use Plan presented here augments these standards to provide additional guidance for new development. Maximum densities contained in this Plan shall be balanced against topographic and natural site constraints and be subject to the General Plan Consistency Review program. The following list contains excerpts of important existing policies and standards that should be emphasized in the review of new development:

General Plan Policies - The General Plan contains goals and policy statements to guide new development. Relevant policies are as follows:

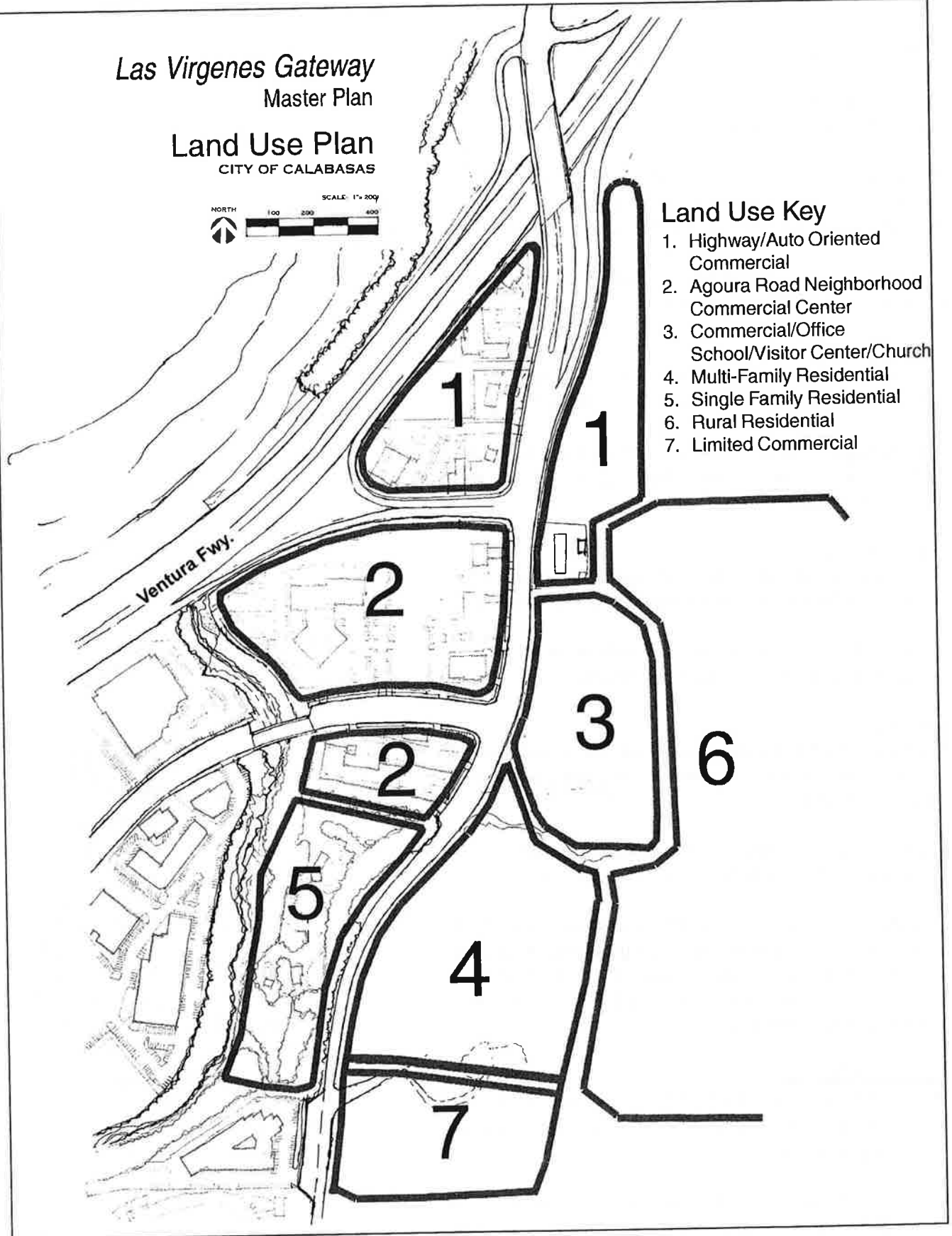
Las Virgenes Gateway
Master Plan

Land Use Plan
CITY OF CALABASAS



Land Use Key

- 1. Highway/Auto Oriented Commercial
- 2. Agoura Road Neighborhood Commercial Center
- 3. Commercial/Office School/Visitor Center/Church
- 4. Multi-Family Residential
- 5. Single Family Residential
- 6. Rural Residential
- 7. Limited Commercial



Open Space

Preserve the view of area hillsides and open ridgelines.

Hillside Management

Maintain the visual character of hillsides.

Minimize the alteration of existing land forms and maintain the natural topographic characteristics of hillside areas, allowing only the minimal disruption required to recognize basic property rights.

Protect the natural character of hillside areas by means of land sculpting to blend graded slopes and terraces with the natural topography.

Preserve all significant ridgelines and other topographic features such as knolls, rock outcrops, canyons and woodlands.

Avoid mass graded "mega pads" for development. Smaller steps or grade changes shall be used over single large slope banks to the greatest extent feasible.

Biotic Resources

Ensure that new development protects riparian areas, oak woodlands, habitat linkages and other biologically sensitive habitats.

Require that construction be separated from sensitive resources through buffers, setbacks, and protective fencing.

Land Use

Emphasize retention of Calabasas' natural environmental setting, semi-rural character and scenic features as a priority over the expansion of urban areas.

Require that new commercial development be compatible the overall semi-rural and residential character of the community.

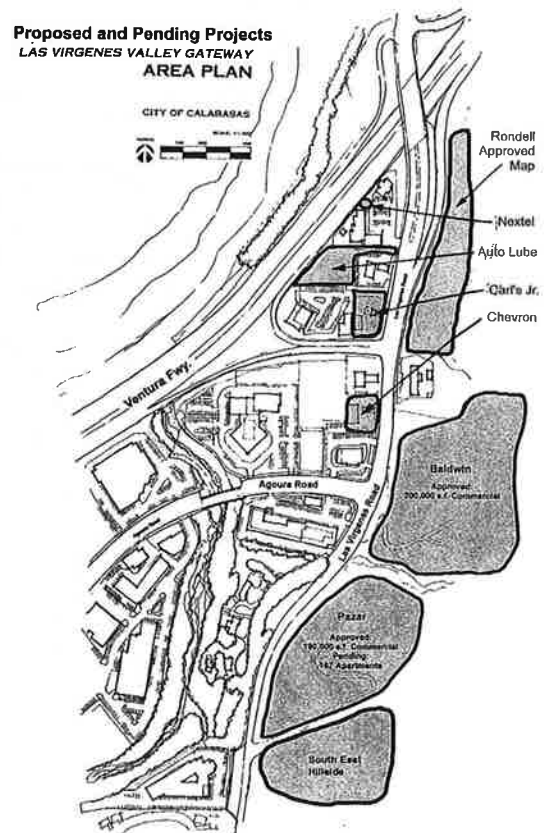
Limit approval of new discretionary development to those which can be integrated into the community, providing for protection of existing residential neighborhoods and desirable non-residential land uses, as well as that which represents the rational utilization of presently uncommitted open space.

Community Character

Maintain the total square footage of structures at a size that maintains the area's open space character and is compatible with the surrounding hillsides.

Encourage the clustering of development to preserve significant environmental features.

“Maintain the visual character of the hillsides.”





Limit the intensity of new business/office parks to that which is consistent with Calabasas' special character and its semi-rural image.

Community Image

View new development not as freestanding features, but as potential additions to an integrated community which must conform to community values and make a positive contribution to the community's quality of life.

Municipal Services and Facilities

Place the ultimate responsibility on the sponsor of new development to ensure that the facilities needed to support the project are available at the time that they are needed.

General Plan Consistency Review Program - Performance standards are provided by which General Plan consistency will be judged

LAS VIRGENES GATEWAY MASTER PLAN RECOMMENDED LAND USE CHANGES (11/12/98)

	EXISTING GENERAL PLAN/ZONING DESIGNATIONS	EXISTING BUILD-OUT POTENTIAL	PROPOSED GENERAL PLAN/ZONING DESIGNATIONS	PROPOSED BUILD-OUT POTENTIAL
RONDELL PROPERTY 4.13 Acres	GP: Hillside Mountainous Zoning: Hillside Mountainous	1 Unit	GP: Business-Retail Zoning: Commercial-Retail LV Overlay Zone	40,000 sq. ft. Highway Commercial and park n' ride or transit center
BALDWIN/VILLAGE PROPERTY 51.4 Acres commercial and 30 Acres residential	GP: Business-Retail with Urban Hillside Overlay and Rural Residential with Urban Hillside Overlay. Zoning: Commercial-Retail Planned Development and Residential-Rural Planned and Development.	Development Agreement for approximately 200,000 sq. ft. of retail commercial.	GP: No Designation Change. Zoning: No Change, Commercial use limitations under the LV Overlay Zone.	Office/Commercial development up to 50,000 sq. ft., 50,000 sq. ft institutional (church/visitor info.) Up to 30 single family residences on 30 acres with residential designation, depending on site/environmental constraints.
PAZAR PARCEL 12 Acres	GP: Business-Retail with Urban Hillside Overlay. Zoning: Commercial-Retail Planned Development.	Development Agreement for 190,000 sq. ft. of commercial development.	GP: Residential-Multiple Family. Zoning: Residential-Multiple Family, Planned Development 12-16 units/acre, LV Overlay Zone.	Up to 144 market rate units or 192 affordable and/or senior units depending on site/environmental constraints.
SOUTH EAST PARCELS 19.84 Acres	GP: Business-Limited Intensity Zoning: Commercial-Limited	171,100 sq. ft. of commercial floor area depending upon site/environmental constraints (FAR 0.2 maximum)	Same as existing	Same as existing
PONTOPPIDAN PROPERTY 7.58 Acres 2-6 du's/lot	GP: Residential-Single Family Zoning: Residential-Single Family	15 single family residences 15-37 + 25% density bonus for senior or low/moderate income housing	Same as existing	Same as existing
AGOURA ROAD NEIGHBORHOOD CENTER	GP: Business-Retail Zoning: Commercial-Retail	128,240 sq. ft. based on vacant lands of 7.36 acres (FAR 0.2 - 0.4)	GP: Business-Retail Zoning: Commercial-Retail with LV Overlay Zone.	96,180 sq. ft. based on vacant lands of 7.36 acres at FAR 0.3. Uses to be limited to Neighborhood Serving Commercial.
HIGHWAY TRIANGLE	GP: Business-Retail Zoning: Commercial-Retail	22,651 sq. ft. based on vacant lands of 1.3 acre (FAR 0.2 - 0.4)	GP: Business-Retail Zoning: Commercial-Retail with LV Overlay Zone.	16,980 sq. ft. based on vacant lands of 1.3 acres at FAR 0.3. Uses to be limited to Highway/Auto Oriented Commercial.

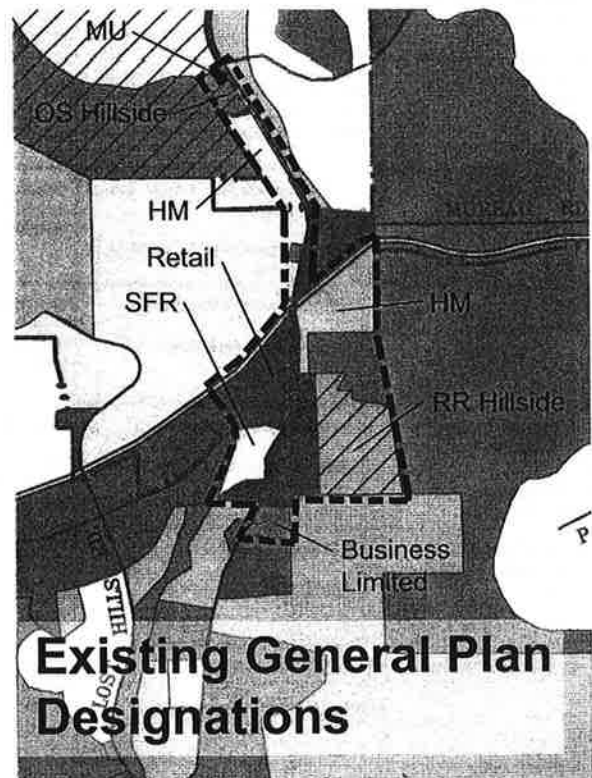
for new development proposals. Relevant standards include Hillside Development, Biotic Resources, Erosion Control, Seismic and Geologic Hazards, Stormwater Management and Flooding, Fire Hazard, Urban Design, and Quality of Life.

Scenic Corridor Ordinance - Las Virgenes Road is a designated scenic corridor. Therefore, all properties located within 500 feet of the road right-of-way and all properties between the right-of-way and the prominent ridge lines are subject to this ordinance. This ordinance specifies the following development guidelines:

- ♦ New development shall underground all utilities.
- ♦ All roofs shall be surfaced with medium dark colored fire retardant non-glare materials and no obtrusive equipment shall be placed on the roof.
- ♦ Colors of fences and walls shall blend with the natural environment.
- ♦ Building setbacks from freeways and open spaces between buildings adjacent to the freeway shall be increased to allow landscaping.
- ♦ Landscaping and tree planting should visually enhance, soften or conceal as much as possible.
- ♦ Lighting shall focus the light directly to the ground to prevent illumination of the night sky and adjacent properties.
- ♦ Parking lots shall be screened with earth berms, landscaping and innovative decorative walls.
- ♦ Grading shall be kept to an absolute minimum. All grading shall be contour graded, gently sculpted and softened to blend with natural contours, and landscaped with environmentally appropriate trees and shrubs.

Calabasas Land Use and Development Code - This document specifies the allowed land uses within each zone district, as well as the height, setback, floor area ratios, site coverage, parking provisions and required permits. This code also contains development standards for access, circulation, Design, Biotic Resources, Drainage, Fences & Walls, Freeway Corridor Development, Hillside & Ridgeline Development, Screening, and Solar Energy.

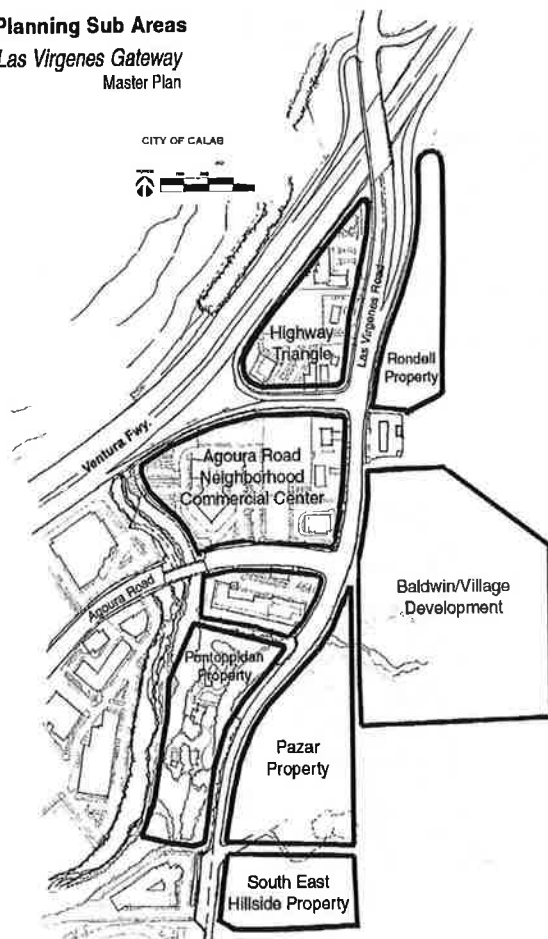
Existing General Plan Land Use Designations - The lands with in the Plan boundary currently have a mix of commercial and residential land use designations that were adopted with the City's General



Plan in 1995. The specific designations are as follows:

- ♦ The Northwest Hillside City lands located north of the freeway and west of Las Virgenes Road are designated Hillside Mountainous. The adjacent County lands that front on Las Virgenes Road are designated for Agricultural use.
- ♦ The neighborhood commercial center on the west just south of the Las Virgenes Road/Thousand Oaks Blvd. intersection has a Mixed Use designation.
- ♦ The lands north of the freeway at Mureau Road are planned for Business Professional Office uses.
- ♦ Immediately south of the freeway, along the east side of Las Virgenes Road, an area referred to as the Rondell Property is designated Hillside Mountainous.
- ♦ The lands on the east side of Las Virgenes Road at the Agoura Road intersection, including the Pazar Parcel and a portion of the Baldwin/Village lands have a designation of Business Retail/Urban Hillside. To the east of these lands are additional Baldwin/Village property with a Rural Residential/Urban Hillside designation.
- ♦ The southernmost parcels on the east side of Las Virgenes Road, referred to as the South East Hillside Property, are designated for Business Limited Commercial.
- ♦ South of the freeway, on the west side of Las Virgenes Road, in an area referred to as the Highway Triangle, the lands are designed Business Retail.
- ♦ The lands along Agoura Road, identified as the Neighborhood Commercial Center in this Plan, are planned for Business Retail land uses.
- ♦ The southernmost lands on the west side of Las Virgenes Road, the Pontoppidan property, are designated for Single Family Residential development.

Planning Sub Areas
Las Virgenes Gateway
Master Plan



Land Use Plan Objectives

When carrying out the Land Use Plan, the following objectives shall be pursued for the listed sub-areas within the Master Plan and adjacent County lands. *All other lands in the Plan area, not discussed below, shall remain under the General Plan and Zoning designations existing at the time the Master Plan is adopted. Except, the Las Virgenes Gateway Overlay Zone shall be applied to all properties in the Plan area.* Future General Plan and Zone change requests can

be considered by the City on a case by case basis when appropriate information is provided by the applicant. Such changes should be judged against the overall objectives of this Plan as well as on-site environmental constraints.

The Highway Triangle - This area includes all properties along the west side of Las Virgenes Road from the freeway to the southbound freeway off ramp.

Highway/auto-oriented commercial uses shall be allowed, under the General Plan Business Retail designation and the Commercial, Retail zone designation. All ground floor uses in this area shall be highway/auto-oriented uses such as auto service, gas stations, mini-marts, fast food convenience stores and restaurants. Upper floor uses can be any use allowed under the Commercial, Retail zone designation. Development intensity shall be limited to a Floor/Area Ratio of 0.3.

New drive-in or drive-through uses where vehicles queue-up with idling engines shall be prohibited, pursuant to the provisions of the City's Development Code.

Reciprocal access and parking plans for internal circulation on contiguous parcels, shall be provided.

Driveways on Las Virgenes Road shall be minimized. Consider shared vehicular access for several parcels.

Telecommunication antennas located in this area shall be of "stealth design," co-located and concealed or integrated into the building.

An entry gateway feature shall be provided at the northeast corner of Las Virgenes Road and the freeway on/offramps (at the Rondell parcel). The northwest corner of this intersection should also receive gateway treatments. This feature shall be consistent with the gateway plan presented in Chapter 9.

The Rondell Property - This 4.1 acre area located on the east side of Las Virgenes Road on the north side of the Mobil gas station is currently designated Urban Hillside but due to Las Virgenes Road frontage and the moderate topography adjacent to the road, could be developed with highway-oriented commercial uses. A General Plan Amendment and Zone change to Business-Retail will be necessary to implement this portion of the Land Use Plan.

Limited retail/highway/auto-oriented development shall be allowed consistent with the General Plan Business-Retail designation and the Commercial, Retail zone designation. The allowed uses shall include hotel/motel uses. Development intensity shall be limited to a

Land Use Limitations

Las Virgenes Gateway
Master Plan

Add the LU Overlay Zone for the
Highway Triangle
Highway/Auto Oriented Uses
(FAR 0.3)



Land Use Designation Changes

Las Virgenes Gateway
Master Plan

Rondell Property:
Existing G.P.: Hillside Mountainous
Existing Zoning: Hillside Mountainous

Proposed G.P.: Retail
Proposed Zoning: Retail (0.2 FAR)
LV Overlay Zone



Floor/Area Ratio of 0.2 or 40,000 square feet, which ever is less. All uses in this area shall be highway/auto-oriented uses such as auto service, gas stations, mini-marts, fast food convenience stores and restaurants.

A park and ride or mini-transit center is a referred use in the public right-of-way. The safety aspects of automobile ingress and egress shall be examined when such a use is proposed. This park and ride could also serve as a trail head for a public trail to the east.

Driveways from Las Virgenes Road shall be minimized. Consider shared vehicular access for several parcels.

New drive-in or drive-through uses where vehicles queue-up with idling engines shall be prohibited, pursuant to the provisions of the City's Development Code.

An entry gateway feature shall be provided at the northeast corner of Las Virgenes Road and the freeway on/offramps. This feature shall be consistent with the gateway plan presented in Chapter 9.

The Baldwin/Village Development - This land encompasses 138.37 acres on the east side of Las Virgenes Road starting at the Las Virgenes/Agoura Road intersection and encompassing the hillside area to the east. These lands are currently designated Business-Retail and Rural Residential with an Urban Hillside Overlay. However, an entitlement exists for commercial development as granted by Los Angeles County. A General Plan and Zone amendment may be necessary for this property, if the location of proposed development does not coincide with the existing General Plan and Zone boundary lines. Any such changes should be appropriate for the land forms on the property. If an agreement is reached for an exchange of Conservancy Open Space lands for Rural Residential lands with development constraints, a General Plan and Zoning change shall be processed.

An office/commercial development shall be allowed, located at the east extension of Agoura Road consistent with the General Plan designation of Business Retail and the zone designation of Commercial, Retail. Development intensity shall not exceed a Floor/Area Ratio of 0.2 or 50,000 square feet, which ever is less. In this area, office use shall encompass up to 75% of the building(s) floor area. This lower FAR is proposed to minimize development in this hillside area and provide a transition to residential and open space uses.

A park visitor center and staging area for access to open space/protected lands is a preferred use and shall be provided in any new commercial development, if feasible.

Land Use Limitations

Las Virgenes Gateway
Master Plan

Baldwin/Village:
Add the LV Overlay Zone for Commercial
Limitations to Restrict the Following:

- Office Commercial (FAR 0.2/50,000 sf)
- Visitor Center/Church/School
- Rural Residential (1du/acre to remain)



Lands for a church, child care center and/or one school (limited to approximately 350 students) shall be provided, if feasible.

Clustered detached single family residences at a density of one dwelling unit per acre or senior/affordable housing may be done consistent with the City's Development Code. Residential density shall be calculated on the lands not developed for commercial/public or institutional uses.

Driveway access from Las Virgenes Road shall be minimized. Shared vehicular access for several parcels should be provided.

Gateway feature(s) shall be provided at the corner of Las Virgenes Road and the extension of Agoura Road. This treatment shall include stone monumentation, landscaping and open space consistent with the gateway feature plan in Chapter 9.

All existing on-site oak trees shall be retained to the greatest extent feasible.

Development shall respect the topography by stepping up and down the slopes. The development shall also blend with the hillside through a variety of massing and the use of muted earthtone colors. Buildings shall be sited to provide a view corridor through the site to the background hills as seen from the Las Virgenes Road freeway overpass, looking south.

Large retaining walls (over 6 feet in height) shall be avoided. Retaining walls shall be either reinforced earthen wall construction (or similar technology), or faced with stone. Along the Las Virgenes Road frontage, retaining walls shall not be located at the sidewalk level. The slope in this area should be split by a retaining wall located halfway up the slope.

The Pazar Property - This land encompasses 12 acres on the east side of Las Virgenes Road just south of the end of Agoura Road. This land is currently designated Business-Retail with an Urban Hillside Overlay. An entitlement exists on this parcel for commercial development as approved by Los Angeles County. A General Plan Amendment and Zone change to Residential Multiple-Family Planned Development is recommended for this property.

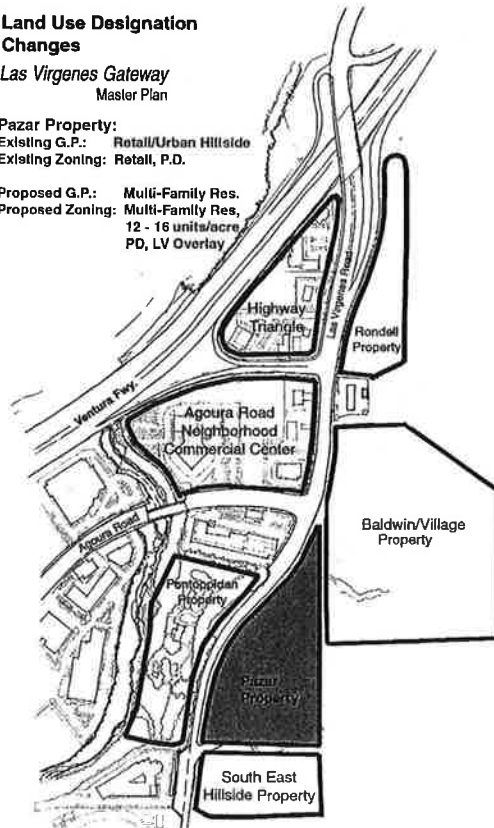
Clustered detached single family or attached multi-family residences shall be allowed at a density of 12 units per acre under the General Plan Residential Multiple-Family designation and the Residential, Multi-Family, Planned Development zone designation. Senior or affordable housing may also be provided for an overall density up to 16 units per acre per the City's Development Code.

Land Use Designation Changes

Las Virgenes Gateway
Master Plan

Pazar Property:
Existing G.P.: Retail/Urban Hillside
Existing Zoning: Retail, P.D.

Proposed G.P.: Multi-Family Res.
Proposed Zoning: Multi-Family Res,
12 - 16 units/acre
PD, LV Overlay



Land Use Designation Changes

Las Virgenes Gateway
Master Plan

Pontoppidan
Single Family Res > Multi Family Res
(12 to 14 units per acre)



Driveway access shall be minimized from Las Virgenes Road. Shared vehicular access for several parcels shall be provided, as feasible.

A gateway feature shall be provided at the corner of Las Virgenes Road and the extension of Agoura Road. This feature shall include stone monumentation, landscaping and open space consistent with the gateway feature plan in this document.

All existing on-site oak trees shall be retained to the greatest extent feasible.

Development shall respect the topography by stepping up and down the slopes. The development shall also blend with the hillside through a variety of massing and the use of muted earthtone colors. Buildings shall be sited to provide a view corridor through the site to the background hills as seen from the Las Virgenes Road freeway overpass, looking south.

Large retaining walls (over 6 feet in height) shall be avoided. Retaining walls shall be either reinforced earthen wall construction (or similar technology), or faced with stone. Along the Las Virgenes Road frontage, retaining walls shall not be located at the sidewalk level. The slope in this area should be split by a retaining wall located halfway up the slope.

Land Use Designation Changes

Las Virgenes Gateway
Master Plan

Southeast Hillside:
Existing G.P.: Limited Business
Existing Zoning: Limited Commercial

Proposed G.P.: Single Family Res
Proposed Zoning: Single Family Res
(2-6 units/acre)
PD, LV Overlay Zone



The Pontoppidan Property - This property includes 7.58 acres on the west side of Las Virgenes Road south of Agoura Road. This land is currently designated for Single Family Residential uses. At the request of the property owner, no land use amendment is recommended at this time. When development is proposed for this property, a multi-use trail should be considered along the creek. This trail would connect with the residential neighborhood to the south and the commercial area to the north.

South East Hillside Property - This area includes 19.8 acres between the Pazar Property and the Water District headquarters. This land is currently designated Business-Retail with an Urban Hillside Overlay and zoned Commercial-Limited. No land use designation change is recommended at this time.

Driveway access shall be minimized from Las Virgenes Road. Shared vehicular access for several parcels shall be provided, as feasible.

A secondary gateway feature shall be provided along Las Virgenes Road on these property's frontages. This feature shall include stone monumentation, landscaping and open space consistent with the gateway feature plan in this document.

All existing on-site oak trees shall be retained to the greatest extent feasible.

Development shall respect the topography by stepping up and down the slopes. The development shall also blend with the hillside through a variety of massing and the use of muted earthtone colors. Buildings shall be sited to provide a view corridor through the site to the background hills as seen from the Las Virgenes Road freeway overpass, looking south.

Large retaining walls (over 6 feet in height) shall be avoided. Retaining walls shall be either reinforced earthen wall construction (or similar technology), or faced with stone. Along the Las Virgenes Road frontage, retaining walls shall not be located at the sidewalk level. The slope in this area should be split by a retaining wall located halfway up the slope.

The Agoura Road Neighborhood Commercial Center - This district encompasses the parcels bordering the west side of Las Virgenes Road between the southbound freeway off-ramp and Agoura Road as well as the seven acre vacant auto dealership parcel. (Parcels on both sides of Agoura Road, east of the creek are within this district.)

A neighborhood center with a lively environment for eating, shopping and socializing is the preferred land use. A neighborhood commercial center shall be allowed under the General Plan designation of Business-Retail and the Commercial, Retail zone designation. The development intensity shall be limited to a Floor/Area Ratio range of 0.2 to 0.4. All uses in this area shall be neighborhood serving uses including the following preferred uses: Grocery store, pharmacy, bookstore, coffee shop, ice cream/yogurt shop, library annex, restaurants, deli, medi-center, community center and day care. A limited amount of hotel/motel uses shall also be allowed.

No more than one gas station and two fast food chain or franchise restaurants shall be permitted in this district.

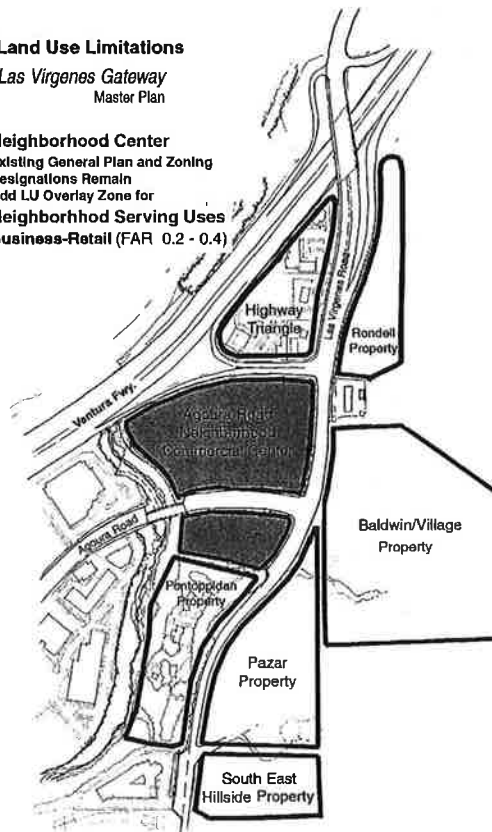
Public and retail/service-oriented uses shall be provided on the ground floor. Upper level uses can be semi-private in nature (e.g. office, professional use).

New drive-in or drive-through uses where vehicles que-up with idling engines shall be prohibited, pursuant to the provisions of the City's Development Code.

Driveways on Las Virgenes Road shall be minimized and wherever feasible joint driveways shall be used to serve several properties.

Land Use Limitations
Las Virgenes Gateway
Master Plan

Neighborhood Center
Existing General Plan and Zoning
Designations Remain
Add LU Overlay Zone for
Neighborhood Serving Uses
Business-Retail (FAR 0.2 - 0.4)



A portion of new development shall orient to the creek with outdoor patios, plaza area, or creekside park.

Examine the feasibility of providing a library annex in this area.

Telecommunication antennas located in this area shall be of "stealth design," co-located and concealed or integrated into the building.

Development of the former auto dealership property shall include construction, funding and/or dedication of significant public improvements to further the Master Plan (e.g. pedestrian road crossing, creek reclamation, creekside park, library annex.)

A portion of the development on the auto dealership property shall front onto Agoura Road to establish a lively sidewalk environment for pedestrians.

Pedestrian and vehicular access shall be provided between the auto dealership parcel and the adjacent parcels that front on Las Virgenes Road.

A public view corridor shall be maintained into the former auto dealership site from Agoura Road.

A public view corridor shall be maintained to the creek from within the auto dealership property.

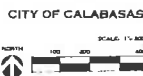
The Northwest Hillside - This district encompasses a small mixed use parcel in the City and all lands outside the City limits on the west side of Las Virgenes Road, north of the Ventura Freeway. It is recommended that the County lands be annexed to the City.

The Mixed Use parcel shall retain the existing mixed Use designation.

Future development of County lands should provide approximately 10 acres for a school and associated playing fields. Or, at a minimum, 10 acres for only sports playing fields.

On County lands, clustered single family residences should be allowed at a density of 2 - 6 dwelling units per acre or senior housing at an appropriate density under the General Plan Rural Residential-Single Family designation and the Rural Residential, Planned Development zone district. Residential density shall be calculated on the lands not developed for school/recreational use.

Northwest Sub-Area
Las Virgenes Gateway
Master Plan



LAS VIRGENES GATEWAY OVERLAY ZONE

The important site, neighborhood and environmental objectives listed above require particular attention in project planning and therefore they are addressed in the Master Plan by an Overlay Zone. The *Las Virgenes Gateway Overlay Zone* (LV Overlay Zone) is established to provide guidance for development and new land uses in addition to the standards and regulations of the underlying zoning districts as established in the City's Development Code. Where a conflict arises between the regulations of the underlying zone district and the LV Overlay Zone, the LV Overlay Zone will take precedence. The Overlay Zone provisions are outlined below but must be adopted by Ordinance in order to become effective.

The LV Overlay Zone is intended to apply to all properties in the Master Plan area. The applicability of the LV Overlay Zone to each specific parcel is shown on the maps in Appendix C.

Purpose - It is the purpose of this Overlay Zone to ensure consistency with the Las Virgenes Gateway Master Plan's land use plan, development standards and design standards. It is the intent also to prevent destruction of the natural beauty, open spaces and environment; to create a memorable gateway to the western portion of the City, the Santa Monica Mountains and Malibu Creek State Park; to protect and enhance private investment; and to protect and enhance the public health, safety and welfare.

Application - The LV Overlay Zone applies to all lands covered in the planning area addressed in the Las Virgenes Gateway Master Plan. The LV Overlay Zone shall be applied to the designated properties through a rezoning (an amendment to the Calabasas Zoning Map) and may be combined with a change in the underlying zoning district. All lands with the LV Overlay will also have an underlying base zone district.

Allowed Land Uses - The allowed land uses in the LV Overlay Zone area are as indicated below. The location of each land use/zoning designation is indicated on the Master Plan Land Use Map and specified in the Land Use Plan Objectives.

Permit Requirements - The permit requirements for development within the LV Overlay zone are the same as provided in the underlying zoning district.

Development Standards - All development within the LV Overlay Zone shall comply with the applicable provisions of the City's Per-

formance Standards for Hillside Development, Freeway Corridor Design Guidelines and Urban Design Guidelines and Sign Standards of the General Plan Consistency Review Program. All development shall also comply with all applicable provisions of the City's Development Code, the Scenic Corridor Ordinance and the Las Virgenes Gateway Master Plan. Additionally, the following special development standards shall apply:

No development shall be located on the east or west hillside areas along Las Virgenes Road at an elevation that is greater than half the height of the top of the ridgeline or backdrop hillside.

Vehicular access points onto Las Virgenes road shall be minimized. When another access point is available, no access shall be allowed onto Las Virgenes Road.

Vehicular parking areas shall be designed for efficiency and safety and shall incorporate reciprocal access and parking arrangements with adjacent properties, unless found to be infeasible.

A vegetative buffer zone is identified for all portions of Malibu Creek. A setback of 30-feet is required from the top of bank for all structures and paving. Natural surfaced trails and fencing can encroach into this setback. A setback of 50-feet is established for all habitable structures.

Required Findings - Approval of any new development or renovation of existing property shall require that the review authority make all the following findings, in addition to the findings required by Chapter 17.62 (Permit Approval or Disapproval) of the City's Development Code:

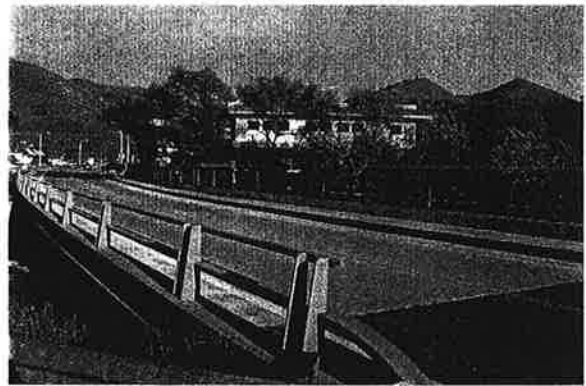
1. The proposed project complies with or accommodates the public improvement plan in the Las Virgenes Gateway Master Plan; and
2. The proposed project incorporates design measures to ensure maximum compatibility with the rural vision and theme of the Las Virgenes Gateway Master Plan; and
3. The proposed project incorporates architectural and landscaping elements that enhance the gateway; and
4. The project's vehicular access and parking plan minimizes conflicts and promotes efficient internal circulation and shared use of facilities wherever feasible.
5. "Preferred" land uses as specified in the Land Use Plan Objectives have been incorporated into the project, as feasible.



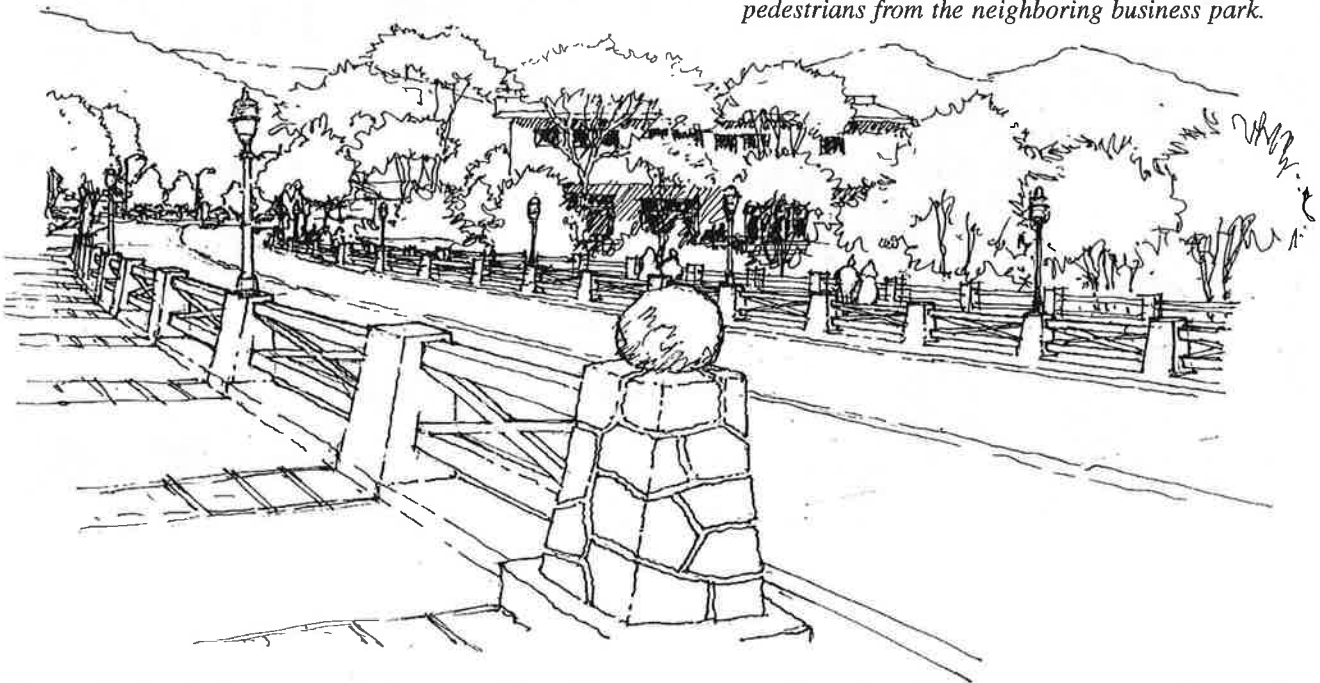
CONCEPTUAL IMAGES OF MASTER PLAN COMPONENTS

It is possible to build projects, both public and private, which are in harmony with the environment and character of the place of Calabasas. Architecture and site improvements which seek to compliment the natural setting - through the use of materials, colors and forms which are complimentary to the rural setting - can help preserve the character of the place while also providing the residents with needed services.

The illustrations provided in this chapter are not intended to specify actual projects to be built or actual site plan layouts. These vignettes do not bind the City or property owners to building specific projects. However, the conceptual plans should provide inspiration and be considered in the review and approval of project applications.



The Agoura Road bridge as it crosses Malibu Creek presents offers little enjoyment for potential pedestrians from the neighboring business park.



Community Standards, or a standard community? Public improvements often establish the community's level of expectation, and can remind us that we live in a unique and special place.



Much of the existing development in the project area was developed without consideration for the area's history and rural context.

"The architecture of new and remodeled buildings reflects the rural character of traditional Southern California." This quote from Chapter Two, "The Vision and Theme," expresses the community's desire to see development which is regional in its origin, respectful of the area's history, climate and rural character.



The same project as it might appear if remodeled in conformance with the goals and guidelines of this Master Plan.



Las Virgenes Road has suffered without a long term vision to guide both private and public changes. (Las Virgenes Road at the 101 Freeway interchange, looking north.)

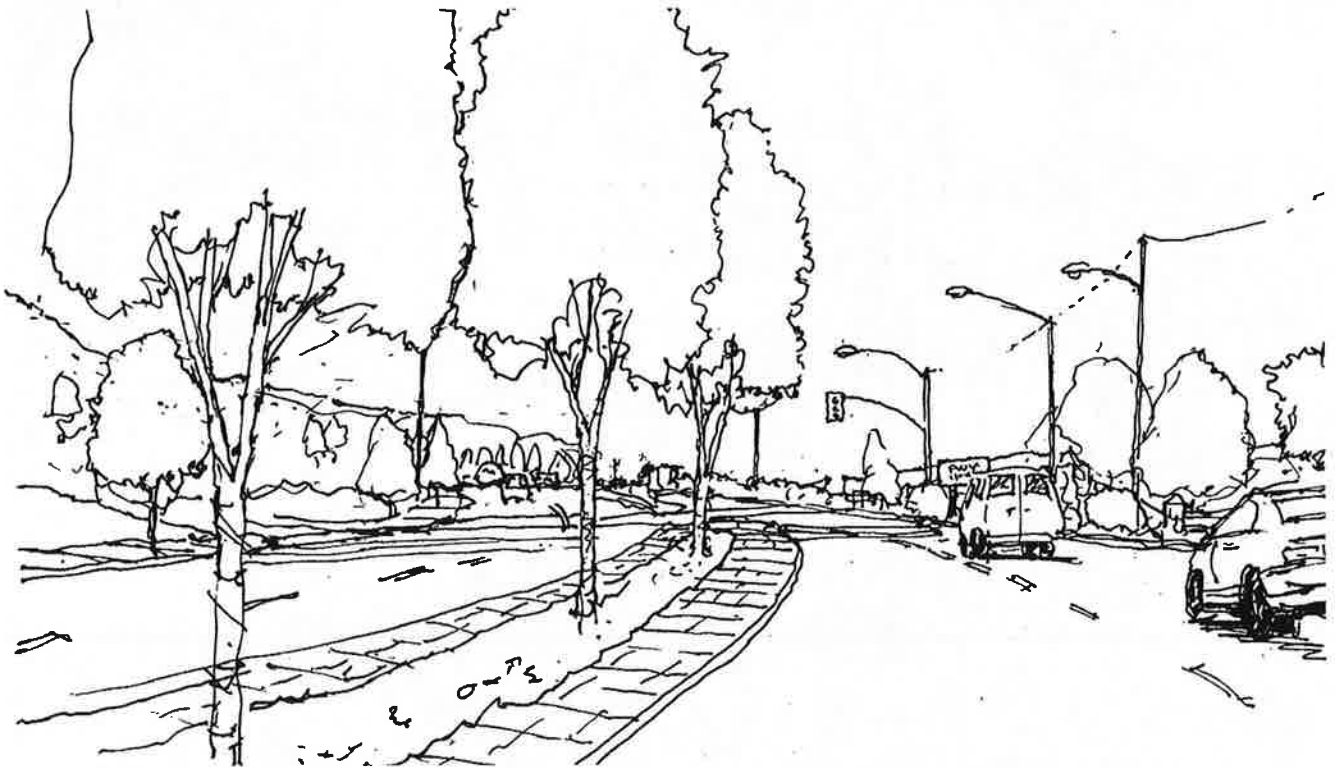


For the Las Virgenes Road area, it is the General Plan's intent that the natural hillsides dominate the freeway corridor at the Las Virgenes road interchange.

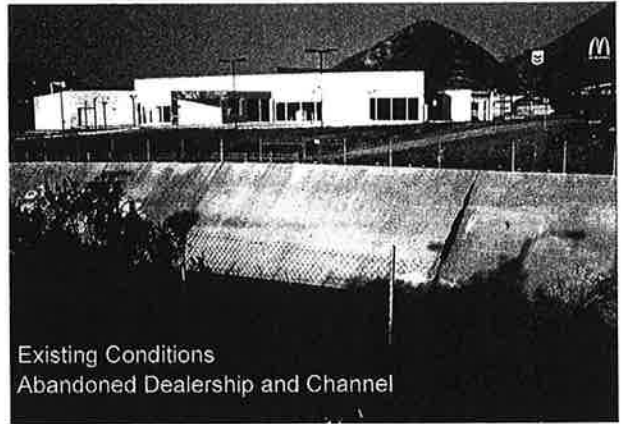


At present, the dominant visual feature at this important entrance to the City is asphalt. (Las Virgenes Road at the 101 Freeway interchange, looking south.)

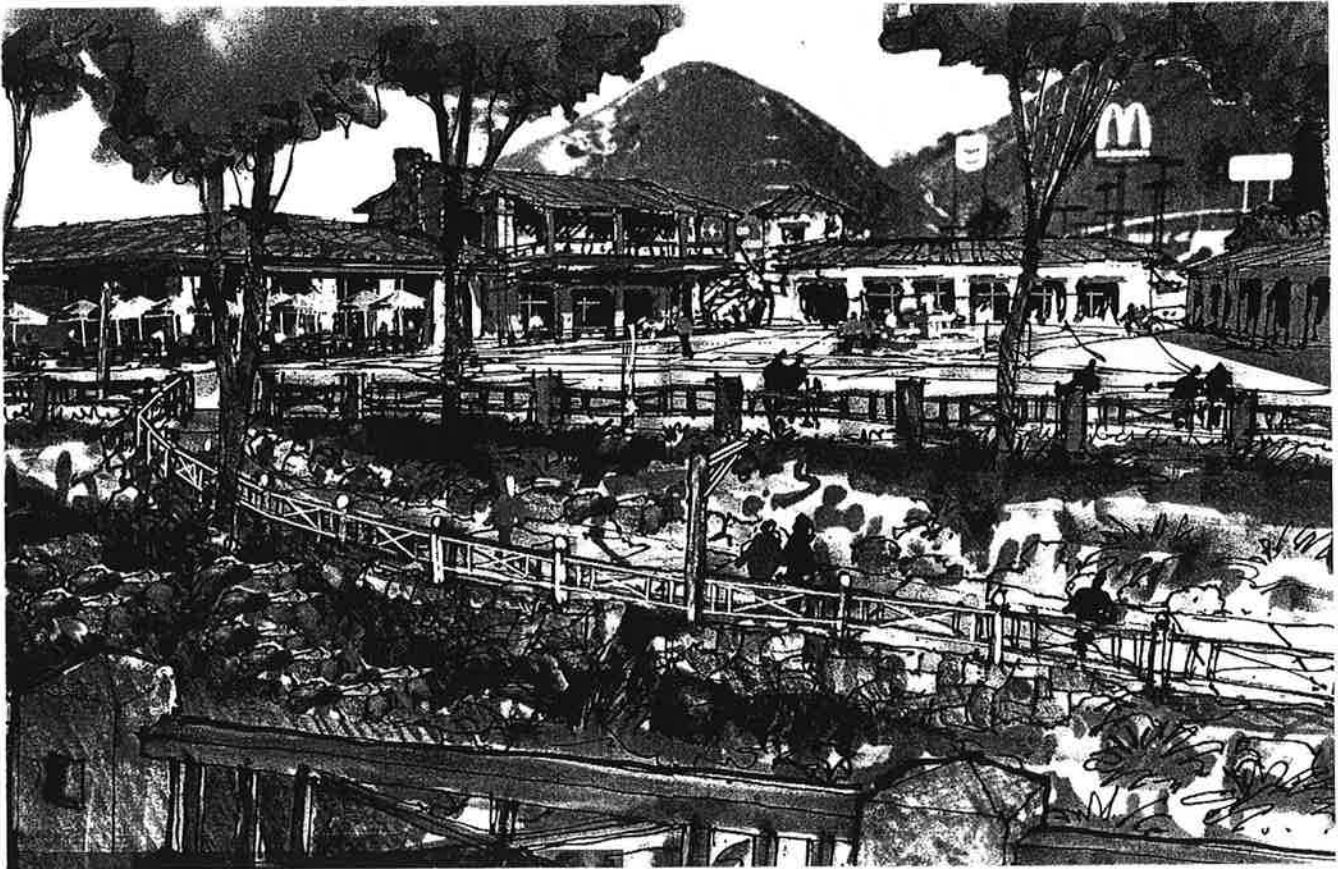
“In the General Plan process, Las Virgenes Road was identified as a “scenic corridor” and the City developed special regulatory measures to promote protection of its scenic qualities.”

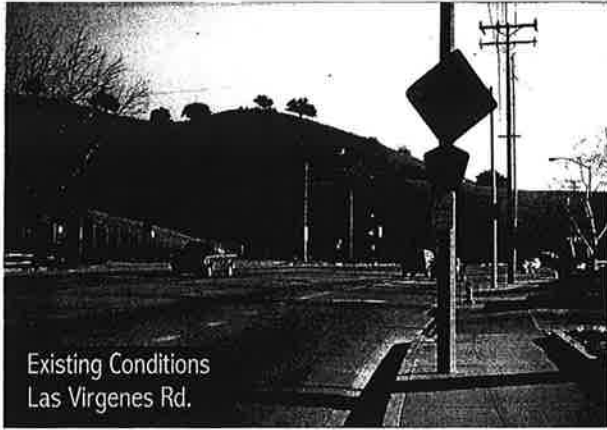


The freeway bridge provides an opportunity to link the north and south areas of the Master Plan in a positive way.

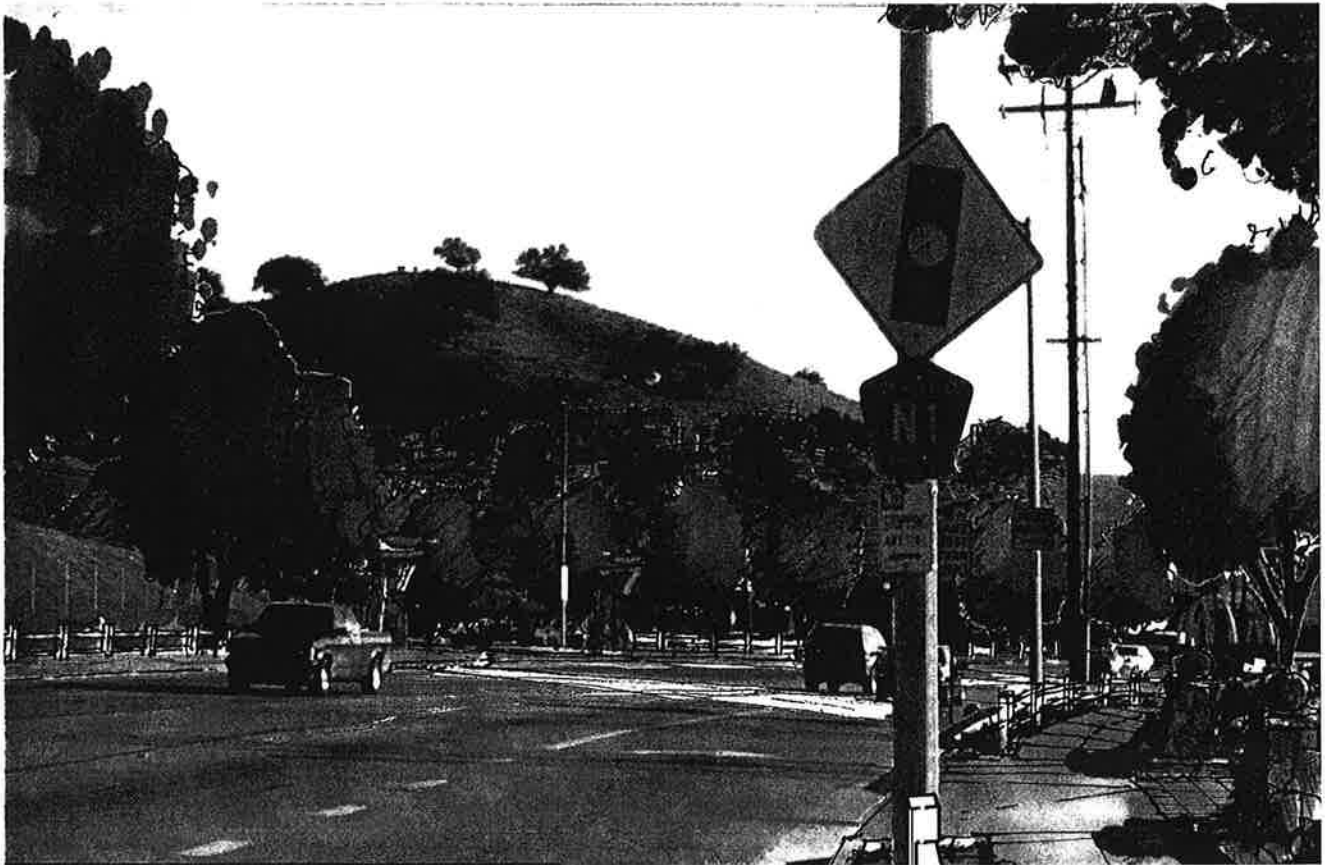


Existing Conditions
Abandoned Dealership and Channel





Existing Conditions
Las Virgenes Rd.



6

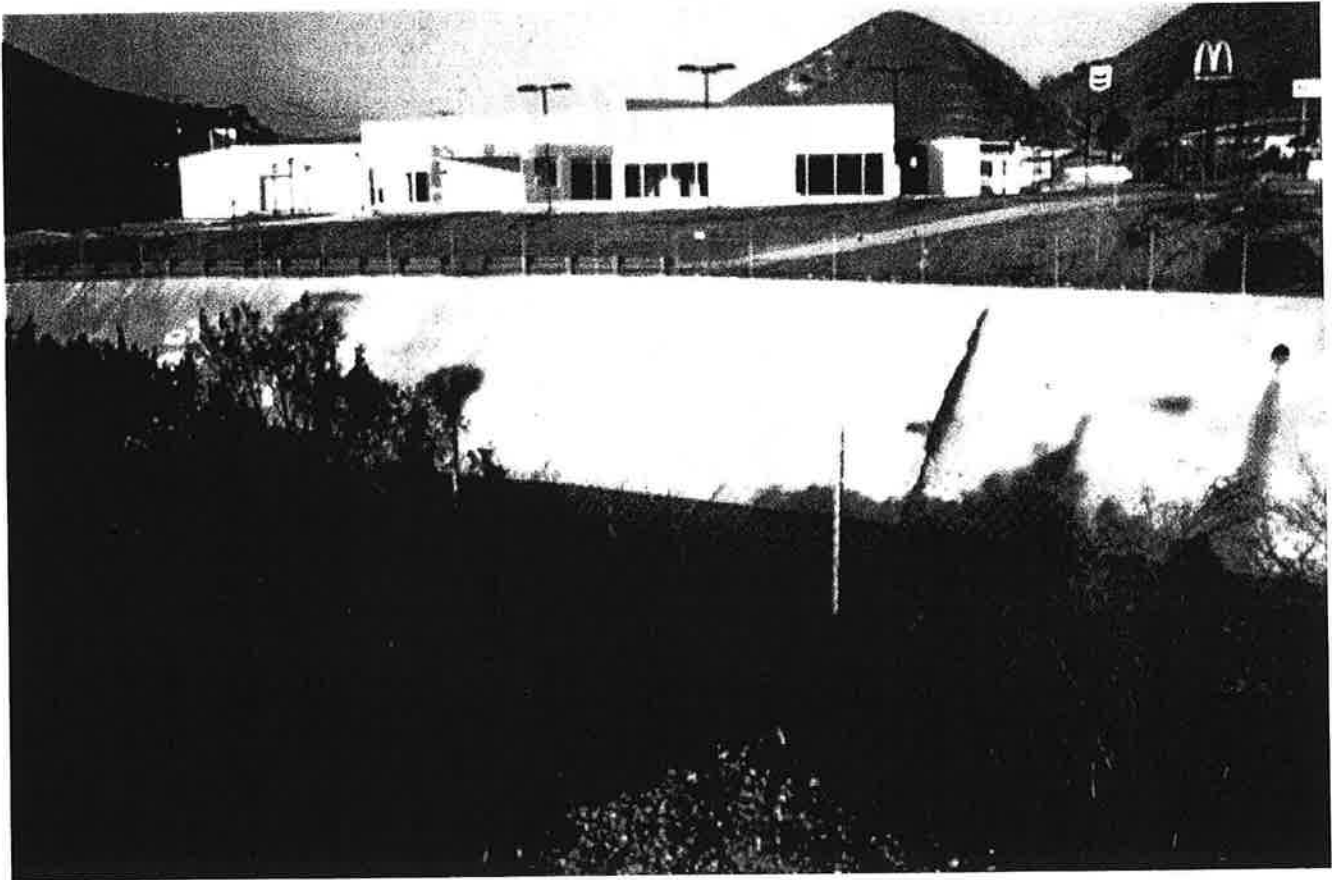
Las Virgenes Creek Reclamation Plan

LAS VIRGENES CREEK RECLAMA- TION PLAN

INTRODUCTION

Las Virgenes Creek runs along the western boundary of the Master Plan area, south of the Ventura Freeway. This creek is a tributary of Malibu Creek and flows from north of Highway 101 southward to join Malibu Creek just outside the Planning Area.

Las Virgenes Creek is an important natural resource in the Master Plan area. The creek segment south of Agoura Road is in a fairly natural state. The creek banks and bottom are natural soil with large stands of willows and sycamores lining the corridor. Unfortunately, the portion of the creek just north of Agoura Road has been lined with concrete for flood control purposes. This creek segment is presently a concrete trapezoidal channel, extending north-to-south, and located between the 101 (Ventura) Freeway and Agoura Road bridge. Construction of the concrete channel years ago removed all habitat and fragmented the riparian system associated with this creek corridor. Currently, no riparian habitat is sustained along this concrete channel.

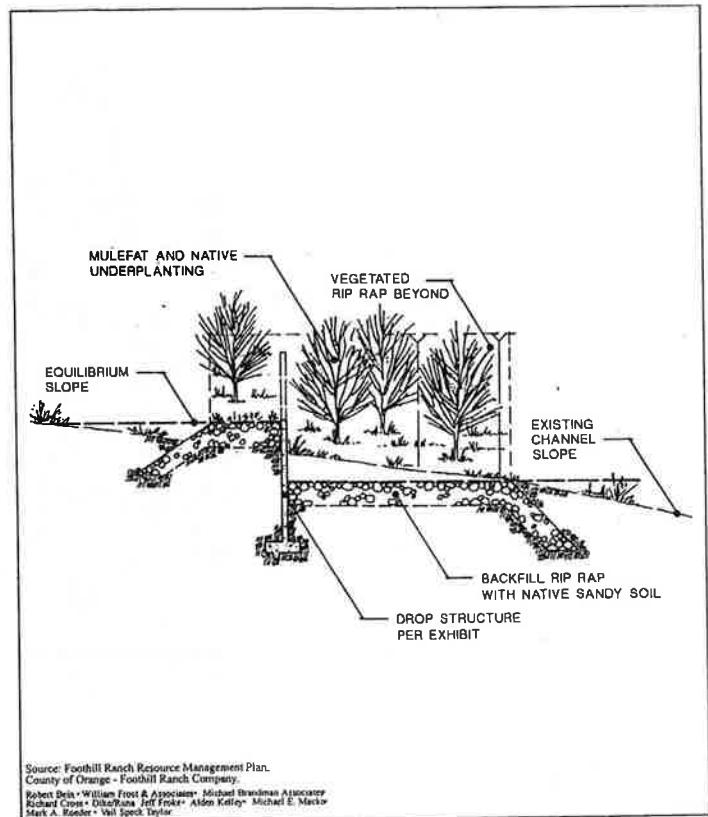


RESTORATION PLAN OBJECTIVES

The main objective of the restoration plan is to reestablish a native creekside habitat to enhance the biological environment and the aesthetics of the Master Plan area. This plan will improve the water quality and provide additional riparian habitat. In addition to providing more native habitat in the region, the restoration would reestablish direct connectivity between the two existing riparian communities to the north and south of the concrete segment. Successful restoration would afford better cover for local wildlife and probably promote increased movement of animals up and down the stream course. Restoration would enhance the greenery and views of an area that is essentially devoid of vegetation.

RESTORATION PLAN COMPONENTS

The potential for restoring the channel, with native riparian plants and establishing a riparian system is excellent. Perennial water flows through this creek segment on an annual basis. This steady supply of water dramatically enhances the successful establishment of riparian plants. The existing slopes are cut back and ideal for restoration. The existing commercial facilities on either side of the subject reach could afford easy access to the channel during construction activities, provided an equitable arrangement can be

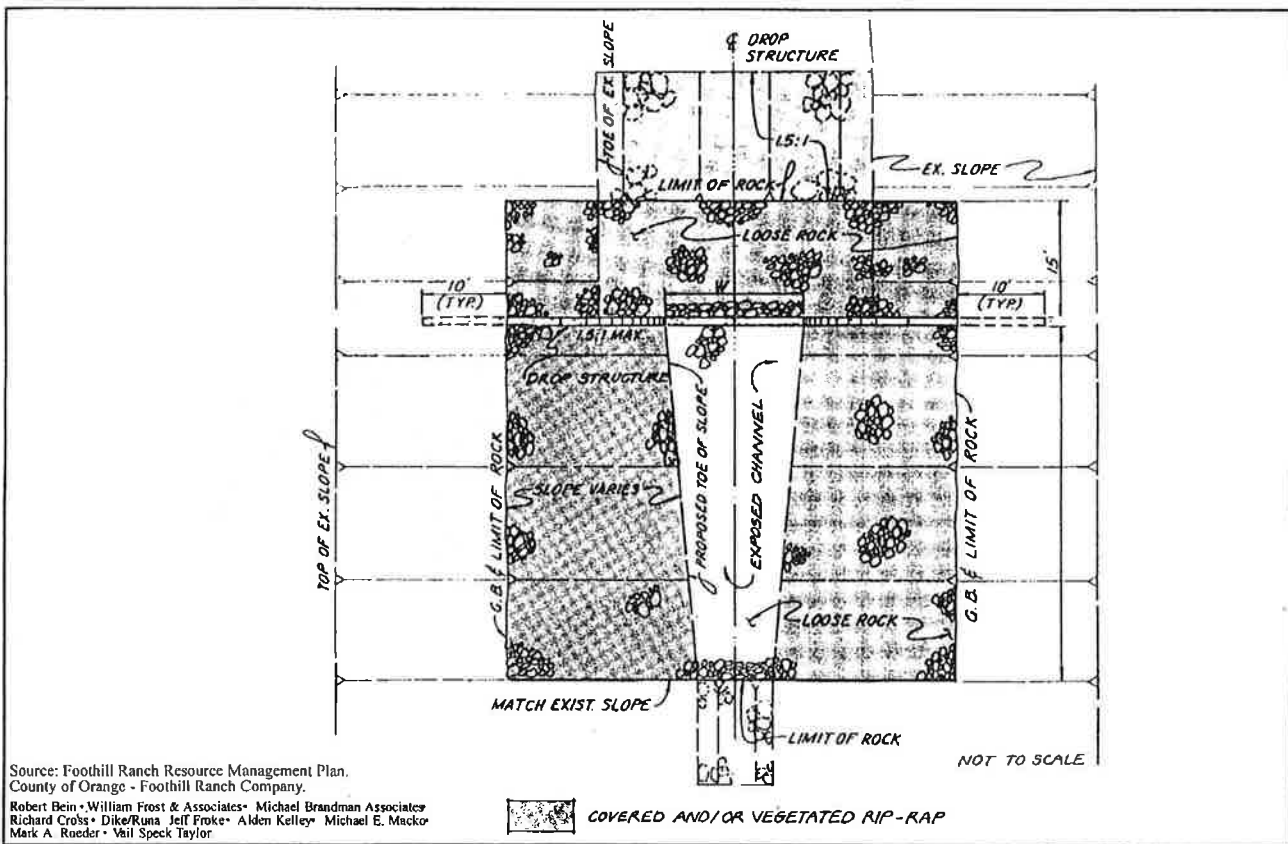


negotiated with the property owner(s). Also, an existing, paved access road/trail, located adjacent and parallel to the east side of the channel, could potentially be used for periodic maintenance activities associated with the restored channel.

Mature willow riparian forests grow immediately upstream and immediately downstream of the concrete segment. This provides an excellent, convenient source of native riparian plant materials (e.g., cuttings) for use in the restoration effort. The availability and use of local plant materials that are adapted to local conditions, increases the likelihood that restoration will be successful, and maintains the genetic integrity of the local ecosystem.

The following recommendations outline a plan for restoration of the Las Virgenes Creek segment between the Ventura Freeway and Agoura Road.

1. First, remove the concrete lining the channel banks and bottom, and over-excavate the banks and bottom of the channel. Establish a slope of 1:1, minimum.
2. Place large riprap along the slopes and channel bottom, and jet soil into the riprap to form a solid foundation for planting native vegetation and to stabilize the streambed and banks. It is important



not to place any kind of liner beneath the riprap. The presence of a liner would result in weak, shallow rooted trees.

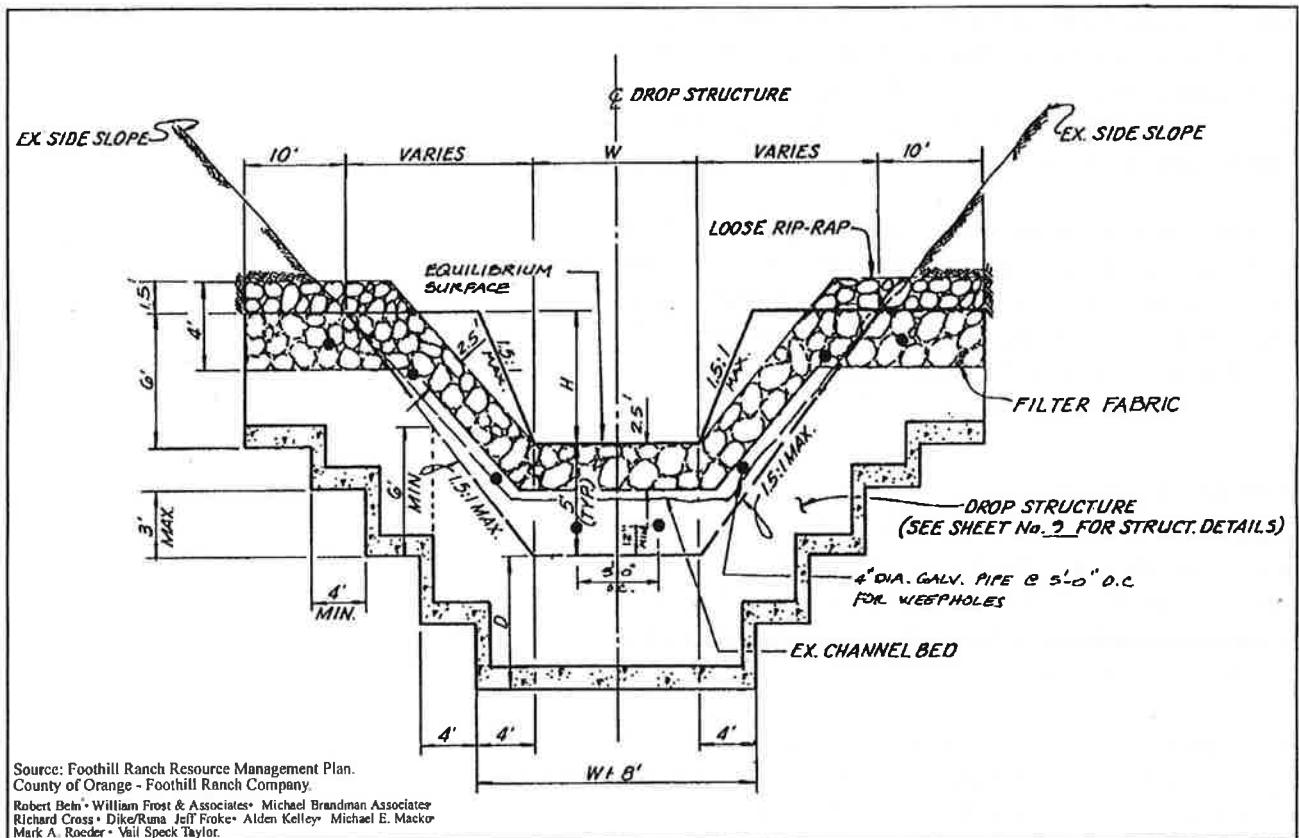
3. Place topsoil over the finished surface prior to planting and seeding. Additional soil may need to be added from time to time, particularly following storm events, until the vegetation is thoroughly established. Allowing some of the riprap to remain exposed will afford the channel a more natural appearance.

4. Provide plantings of native riparian species including the following:

Native riparian trees including sycamore, alder, walnut, cotton woods and willows should be planted to provide shade and enrich the habitat for birds and other wildlife.

The following shrubs, herbaceous plants and vines should also be used in the revegetation program:

- Baccharis pillularis consanguinea (coyote bush)
- Diplacus/Mimulus (monkey flower)
- Rhamnus californica (coffe berry)
- Rubus ursinus (blackberry)
- Heteromeles arbutifolia (toyon)



Ribes speciosum (gooseberry)
Sambucus mexicana (elderberry)
Atemesia (mugwort)
Typha spp. (cat tails)
Equisetum (horse tails)

5. Another option is the substitution of the large riprap with interconnected concrete blocks (e.g., Armorflex®). This would result in the establishment of some riparian habitat and would be preferred over the existing condition; however, this approach does have drawbacks and limitations. Armorflex® type materials include a lining of cable-connected, prefabricated concrete blocks with geogrid anchorage for soil reinforcement. The presence of a liner under the concrete blocks could result in weak, shallow rooted trees. Since the liner prevents deep saturation of the soil, it also prevents deep rooting of trees. Shallow rooted trees are more likely to be uprooted and toppled, especially during heavy storm events. Also, the holes in the concrete blocks restrict the diameter growth of developing trees, thus reducing their overall size and weakening them. This approach may be more costly and time-consuming than the buried riprap approach. Several diagrams of creek bottom and bank restoration plans are illustrated on the following pages.

6. Numerous cliff swallows (*Petrochelidon pyrrhonota*) have been observed nesting under the concrete ledge of the Agoura Road bridge. Since these birds are protected under the Migratory Bird Treaty Act, impacts to their nesting must be avoided. All bridge and creek-related construction, such as widening or other improvements, must be carried out when the birds are not present (i.e., September through February).

7. A vegetative buffer zone shall be provided for all portions of Las Virgenes Creek. A setback of 30 feet is required from the top of bank for all non-habitable structures and paving. Natural surfaced trails and fencing can encroach into this setback. A setback of 50-feet is established for all paving and habitable structures.

CONCLUSION

The recommended method of channel restoration has been implemented successfully in Orange County, and the existing conditions of the Las Virgenes Creek channel are optimal for channel restoration. Several photographs of successful creek restorations are included on the following pages.

Permits from various resource/regulatory agencies (e.g., Army Corps of Engineers, California Department of Fish and Game, Fish and Wildlife Service, Regional Water Quality Control Board, Los

Angeles County Flood Control) will be needed to pursue this restoration plan. Support from these agencies can be expected for a number of reasons. A soft bottom channel will greatly improve the hydrogeomorphic functions, including filtration of pollutants, groundwater recharge, and nutrient cycling. Also, the ecological benefits of establishing native riparian habitat are numerous. Flood control concerns can be accommodated by maintaining appropriate flow characteristics and providing maintenance opportunities.



DESIGN GUIDELINES & STANDARDS

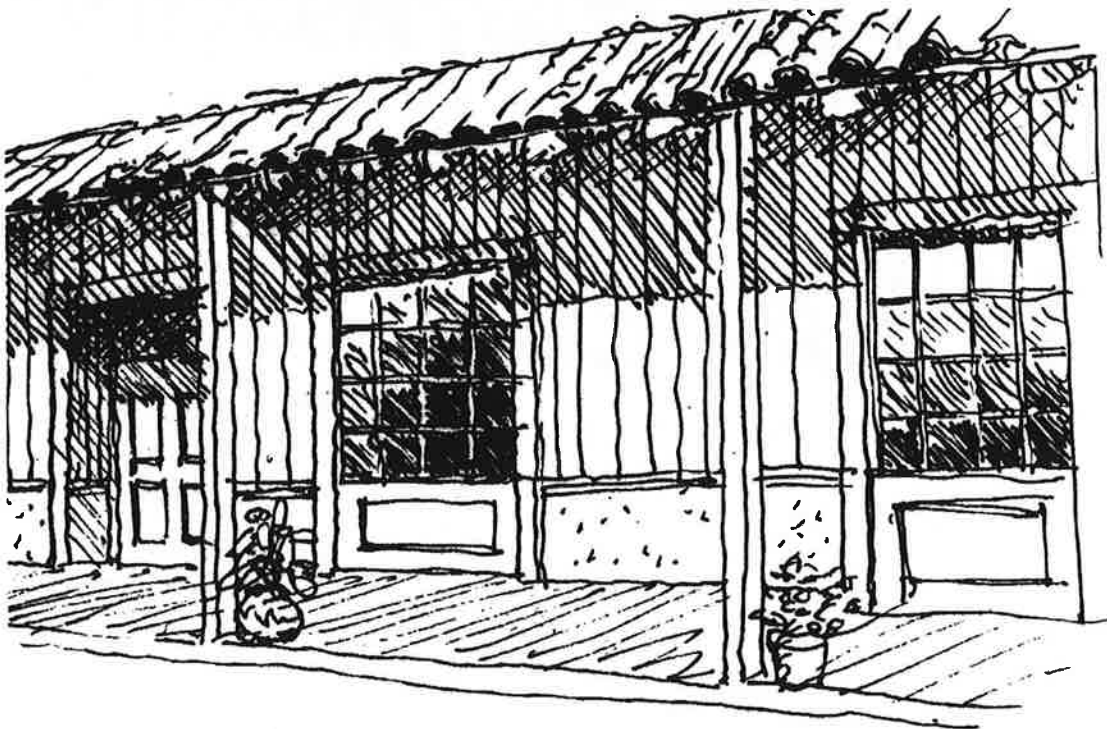
An important function of the Las Virgenes Master Plan is to maintain and enhance Calabasas' character by providing a pleasing visual experience for residents and visitors. Although the improvements in the public right-of-way play an important role in establishing this character, the buildings and site improvements on private property play an equal or greater role in that they create the "backdrop" for the day-to-day use of the place. The following design standards for renovation and new development of private property are aimed at ensuring that the built environment provides an appropriate expression of the community's character and that landscaping is compatible, appropriate and well integrated into all properties. All new development, additions and renovations shall be required to comply with the architectural, landscaping and sign guidelines and standards outlined in this section of the Master Plan.

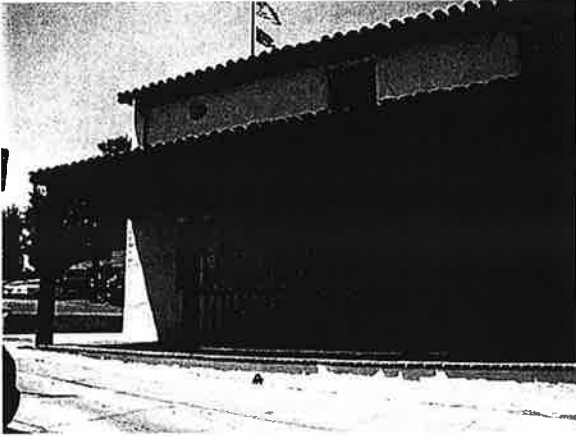


ARCHITECTURAL GUIDELINES FOR PRIVATE PROPERTY

1. Design Objective

One of the primary goals of this master plan is to reestablish the Las Virgenes Corridor as a rural gateway to the community and sur-



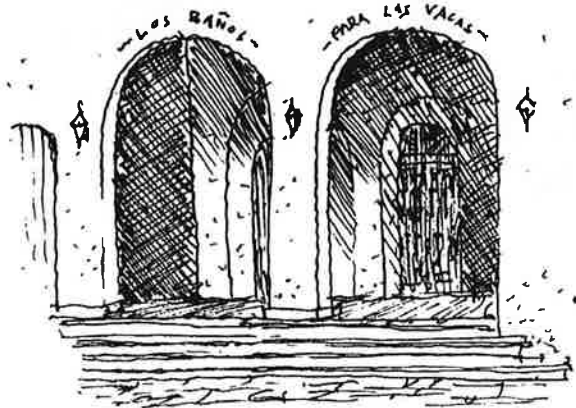


rounding environs. These guidelines are intended to help property owners, developers and architects understand how to implement designs compatible with one of the community's primary objectives of creating a built environment which complements the natural setting.

The design recommendations and requirements provided here are based upon input received from the workshop process, which included residents, business and property owners, as well as developers with an interest in pursuing projects in the planning area.

2. Character, Scale and Building Design

The preferred architectural emphasis which emerged in the community workshops was expressive of a rural theme, rooted in California's history of simple, massive (adobe and timber-based) construction. Although there is latitude for interpretation, the preferred design is decidedly *not Contemporary Mediterranean nor Old Town Western*. Stylistically, the "Monterey" style probably comes closest to reflecting the community's preference, and it is this source that has served as the inspiration for these guidelines. This style is indigenous to the region, as exemplified by the Gillette estate and the Leonis Adobe. This style is also prevalent throughout other regions of California which were colonized by the Spanish Missionaries and settlers in the 18th and 19th centuries, and is quite evident in the San Diego, Santa Barbara and Monterey areas of California.



Design in the Monterey Style

This rural early California architecture is typified by one and two story structures of horizontal mass, with exteriors of wood and plaster. These buildings often include balconies and exterior stairs and walks. The exterior massing and articulation is usually a result of simple forms brought together informally, often asymmetrically, to create a complex, or grouping, of structures. Landscaping is often integral to the building plan, incorporating courts or half courts, garden walls, trellises, arbors and the like.

Buildings influenced by the Monterey Style often appear to have evolved over time, with different "parts" responding to changing or evolving needs of the inhabitants. The expression of these parts can serve to soften or enliven the otherwise plain and simple massing of the building(s).

People often overlook the subtle differences in California's regional architectural styles of Spanish influence, instead lumping them all together as "Spanish-style" design. True Spanish "high architecture" is the product of a refined and powerful European culture. The buildings of Spain's government and upper class in the 1700's



and 1800's were very refined and elegant, and often included ornate iron work, finely hewn stone and decorated tile.

But throughout Mexico and California, there are clear differences in the regional variations of Spanish-influenced architecture, related historically to cultural, climatic and technological constraints which the Spanish and other California Colonials had to deal with as they worked their way up the California coast.

In the Monterey style, the results of these influences are manifest in a somewhat rustic architecture, refined in proportions of wall and openings, but more crude in detail, incorporating heavy timber and coarse wood detailing in lieu of more refined decoration. The result is an architecture which tends to be more relaxed and informal, with deep set windows to keep the hot sun out, and clustered plans to form small courts and protected gardens.

Monterey style architecture often combines wood-framed and woodsided walls (usually at the second floor) with those of heavier adobe and plaster at the ground floor. Roofs were often of wood shake instead of clay tile.



3. Commercial Development

It is the intent of the community that commercial development within the Las Virgenes Gateway Master Plan Area will reflect and compliment the natural, rural and rustic character of the surrounding countryside. Garish, loud, visually busy design which emphasizes commercialism over aesthetics will not be allowed.

Further, it is the community's intent to establish the area as a "village center," serving first and foremost area residents and employees, and secondly, visitors to the area and region. Corporate design, including colors, logos and architectural icons, which may reflect a particular company's or franchises "national image" will only be supported if such design is also in keeping with the goals and intent of the guidelines presented herein.

4. Residential Development

It is the community's desire that residential development, especially when located on hillslopes, should be understated - low in profile and designed to step in conformance with natural topography of the site. Architectural character consistent with these guidelines shall be incorporated into projects.

Colors and materials should be muted and sympathetic with the colors and textures of the terrain and foliage of the site and vicinity. Parking areas should be screened and tucked behind buildings where possible. Retaining walls should be minimized, and should be integrated into the building design wherever possible. In addi-





tion, the following guidelines will apply.

5. Design Guidelines

The following criteria will be used to assess development proposals in the Master Plan area:

A. Site Design

Necessary Elements:

1. Generous setbacks from Agoura Road and Las Virgenes Road;
2. Buildings orient to the street where possible;
3. Parking internalized to the site as part of a court, separated from street and well screened by buildings and landscaping;
4. Edge landscaping to compliment the public improvements;
5. Common vehicular access between adjacent parcels wherever feasible, minimize street curb cuts;
6. Common service access between adjacent parcels wherever feasible;
7. Building Elements combine to create pedestrian spaces (courtyards, paths, etc.) where possible;
8. Pedestrian access from public streets should be pronounced.
9. Projects adjacent to Malibu Creek take special consideration for edge treatment, access to path system and complimentary site design.
10. Shade should be considered a premium amenity, provided by both building and landscape elements.

B. Exterior Building Design

Necessary Elements:

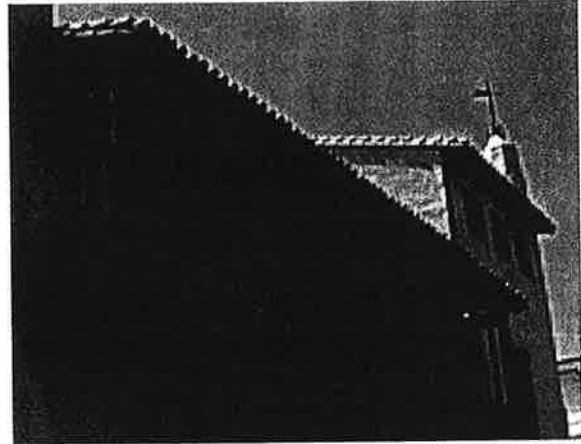
1. Simple building massing; larger buildings result from groupings of smaller mass elements;
2. Building massing tends toward horizontal;
3. Openings for doors and windows tend toward vertical, or groupings of vertical;
4. Roof overhangs are functional for weather and sun protection;
5. Roof massing is simple, roof pitches are low and may vary slightly from mass elements;
6. Openings, especially at entries, are often recessed into wall elements.
7. Clay tile (flat or barrel) roofs are preferred.

C. Architectural Elements

Necessary Elements:

1. Building entries should be readily identifiable, protected by a balcony, arcade, or wall recess;

2. Roofs should range in pitch from 3:12 minimum to 6:12 maximum. Dominant roof pitches should be 3:12 or 4:12 maximum.
3. Balcony elements, either additive (added to exterior) or subtractive (cut out of building mass) can provide interest or emphasis.
4. Windows are generally vertical and sizes may vary. No mirrored glass or glue-on window dividers are allowed in window designs.
5. Support facilities, including utility meters, trash enclosures and mechanical equipment should be accommodated in the building design to "disappear;"
6. Exterior arcades, colonnades and porches provide relief from the simple massing of the buildings.
7. Chimneys and decorative vent stacks can provide relief at the skyline.
8. Exterior stairways can become significant design features, while also announcing the presence of second story tenants.



D. Building Accessories and Details

Necessary Elements:

1. Details, such as decorative lighting, awnings, rails, iron grilles and seat walls, should enliven and compliment the simple forms of the architecture.
2. Decorative Plaster work, in sills, cornices and balconies, add to visual interest;
3. Roof elements, such as rafter tails, rain gutters and down spouts, should be designed to compliment the architecture.
4. Windows and window frames should be substantial, having thickness, color and dimension similar to wood.
5. Mirrored or highly reflective glass should be avoided.
6. Landscape walls, rails and fencing should be designed compatible with the architecture of the building.

E. Materials

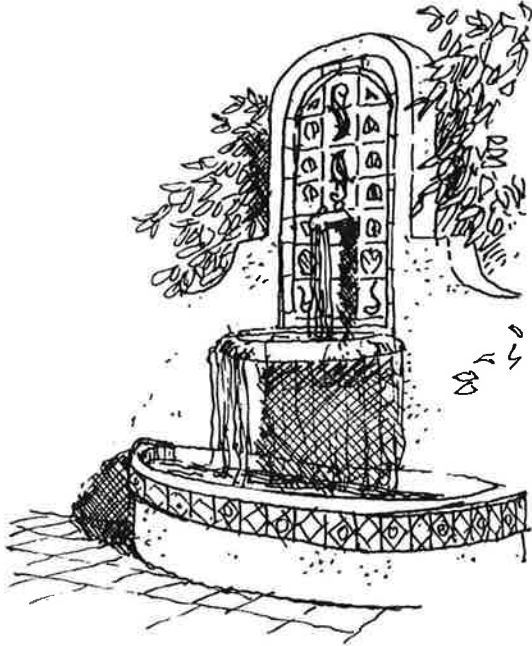
Necessary Elements:

1. Exterior building surface materials should be limited to a maximum of three different types in any design. These should be applied as follows:

walls:

- exterior plaster stucco, hand-finished and light sand or "mission" texture;
- wood siding, shingle, lap or "board and batten", in limited areas;
- stone, brick or block masonry, with appropriate detailing;



**roofs:**

- clay tile (Spanish, mission);
- concrete tile (shake, slate);

railing:

- wrought iron, painted or blackened;
- wood, turned or shaped, stained or painted;

architectural details:

- sills, cornice and similar details of plaster, stone or pre-cast concrete;
- vent grilles, spires and similar details of wrought iron;
- exposed downspouts, scuppers, flashing and related metal work of copper or galvanized iron.

F. Exterior Colors

The natural setting for the Las Virgenes Master Plan Area is rich and varied in color, but the dominant colors are earthen and muted by the nature of the surroundings. Rock outcroppings boast ochers, rust, burnt siennas and sandy browns. Vegetation is of muted greens, olives, browns and occasional mustard yellows.

These guidelines encourage design which will complement and blend with this natural setting, not contrast with it. The following criteria are in keeping with this intent: should be considered as an integral part of building design.

1. Where possible, materials should present their natural or similar color, appropriately finished and sealed to insure long life and aesthetic durability;
2. Larger surfaces should be of muted colors;
3. The use of more intense color, sparingly and as an enhancement to detail, is acceptable;

G. Additions, Renovations and Remodels

Where the original building is considered to be consistent with these guidelines, design of additions and remodels are to be in keeping with the character and detail of the original structure, especially where an historic or potentially historic building is concerned.

Where the existing building does not reflect the rural quality and design detail depicted in these guidelines, care shall be taken to incorporate the entire building into the redesign effort, such that the end project reflects the quality and character described



herein.

H. Architectural Lighting

Lighting should be considered as an integral part of building design. Proper lighting design will have a positive effect on the building and its users, as well as help to promote a safe and enjoyable nighttime pedestrian environment in the plan area. All exterior lighting shall be aesthetic and non-obtrusive.

Types of Lighting

Area Lighting

This is for public and private parking lots, alleyways, parkways and walks. Such lighting should be set in a manner that assures maximum lighting benefit without allowing stray light to intrude into windows of nearby residents or to create glare problems for nearby automobile traffic.

Ornamental Lighting

This is not intended to light large areas or wall surfaces. Ornamental lighting plays an important role in bringing visual life to streets at night. Colors, finishes and design of ornamental lighting should be compatible with the design characteristics of the architecture.

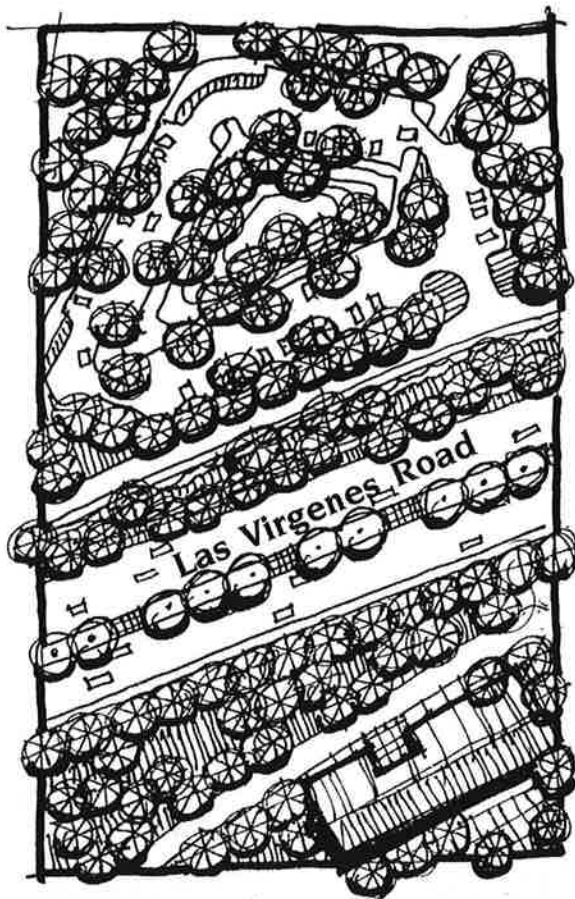
Hidden Source Lighting

For certain prominent architectural or landscape features, hidden source lighting can be used to create dramatic effects, illuminating towers or other unique architectural or site characteristics. Such lighting can be concealed in soffits, behind ledges or parapets, or even set into landscape areas with the light directed at the desired element to be highlighted.

Lighting Design Integration

It is recommended that a lighting design professional be consulted as an integral part of the design process for determining types and intensities of lighting to be used. In addition, the following "rules of thumb" should apply:

- Use the minimum brightness necessary for illumination of large areas.
- Use brighter, more intense lighting to highlight architectural features and focal points.



This vignette of Las Virgenes Road and adjacent development illustrates the enhanced proposed streetscape. Note the trees in the parking lots and the landscape buffer to adjacent development.

LANDSCAPE STANDARDS FOR PRIVATE PROPERTY

1. Design Objectives

Landscaping shall preserve and promote the aesthetic character and value of the Las Virgenes Valley in the following ways:

Plantings shall define, unify and enhance streets, gateways, bicycle lanes, and other elements of the public and private realm.

Plantings shall embellish and enhance private yards, edge conditions, circulation areas and parking lots.

Plantings shall provide significant shade on all sidewalks and open parking and sidewalk areas.

Plantings shall screen views of parking, loading and service yards.

Plantings shall frame but not block views of the ridelines as seen from public view corridors.

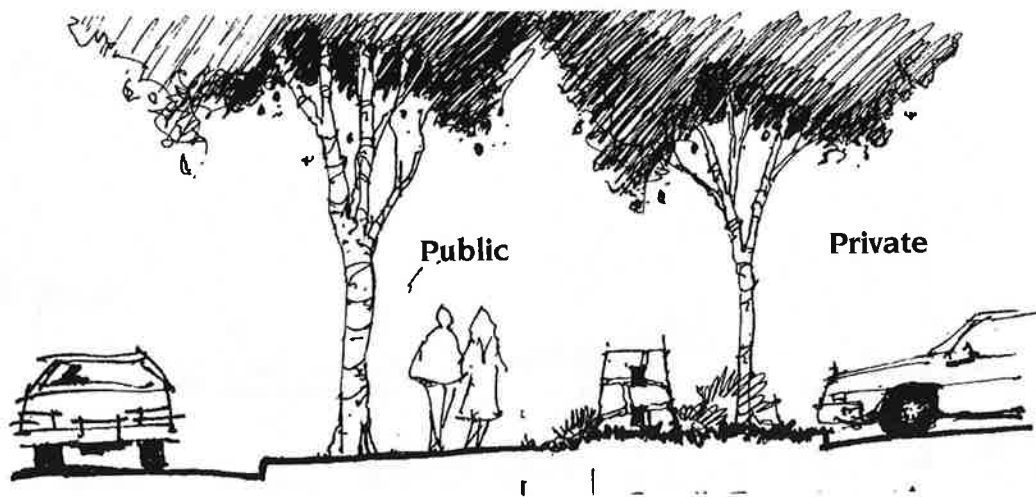
Plantings shall enhance the rural Southern California theme.

2. Planting Design

This section describes the minimum landscape requirements for all improvements on private land within the Master Plan area.

A. Street Frontage

The principal plantings on all street frontages are tree rows, which define the public space. All streetscapes shall be planted

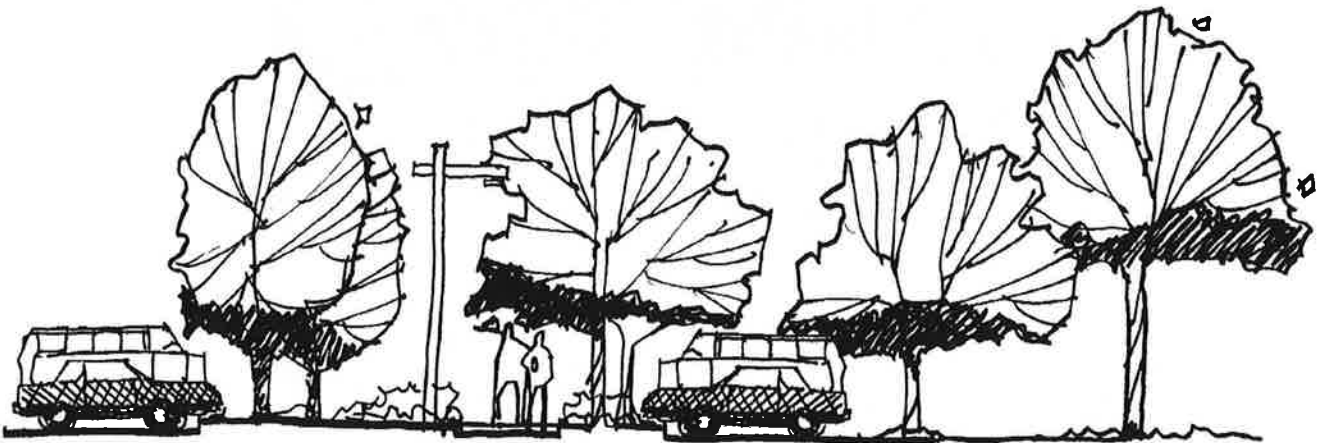
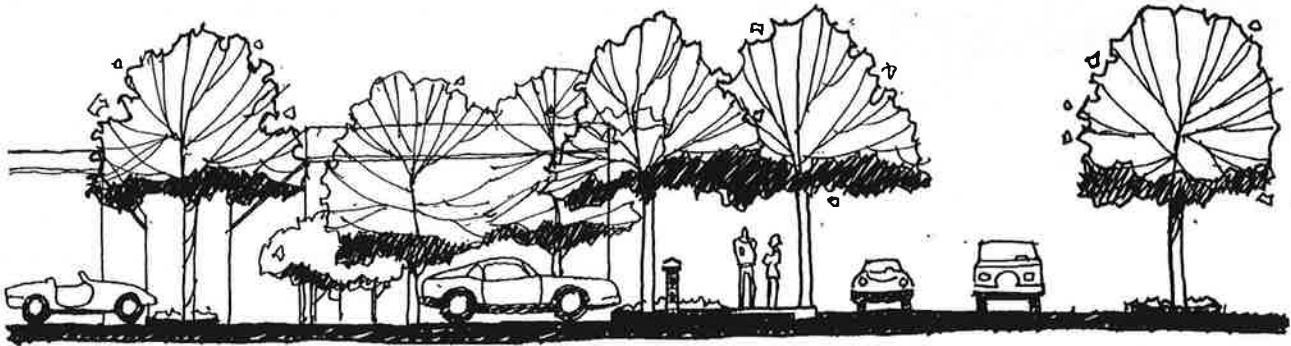


This street section allows for an integrated transition between the public and private realms.

with 36-inch box sized trees as listed in the Public Improvements Chapter of the Master Plan. Consistency in tree species and spacing shall be used to establish a strong street identity. All new and renovation projects shall provide street trees per the Master Plan requirements. These requirements reflect the plans outlined in the *Las Virgenes Road Corridor Plan*:

B. Front Yards (Setback Areas)

Plantings in yard areas fronting on streets shall be appropriate to the scale, orientation, and purpose of the yard. Appropriate plant materials and designs for specific frontage yard types are listed below. Minimum size of tree plantings shall be 24" box, and 33% of the trees shall be at least 36" box size. There shall be a minimum of one tree for every 800 square feet of landscaped area. Ground cover (no turf) and shrubs shall also be planted behind the sidewalk. At facades, low shrubs and/or ground cover shall be planted against the facade. At garden walls and retaining walls, low shrubs and wall vines shall be planted against the wall. Minimum spacing on vines attached to walls shall be 12 feet on center. Shrubs shall be massed and may be configured as maintained hedges. Hardscape shall be



These sections illustrate the condition of a parking lot fronting the street (above) and adjacent parking lots abutting each other (below). Note the minimum 50% shade coverage afforded by fully mature trees.

used to accent entrances and in public plaza or open yard areas.

C. Other Yards

Rear yards and property lines that abut the Ventura Freeway shall be landscaped with trees and shrubs. Appropriate species are listed below. Open yard areas for residential uses may be planted with turf.

D. Parking Lots

Landscaping of parking lots shall consist of 36-inch box sized trees, which shall be provided at a minimum rate of one tree per four parking spaces. Trees shall be the predominant plant material in the parking lots and shall be capable of creating 50% shade upon the total paved surface when mature. Trees shall be round-headed canopy type trees and may be evergreen or deciduous. Trees shall be supplemented with ground covers and shrubs as listed below.

3. Additional Standards

Additionally, the following landscape standards shall apply in the Master Plan area:

- ♦ All areas not devoted to paving or building shall be landscaped and permanently maintained.
- ♦ All landscaped areas are to be delineated with minimum 6-inch concrete curbs.
- ♦ Permanent automatic irrigation facilities shall be provided in all landscaped areas. Moisture sensing devices and water monitoring devices shall be incorporated into the irrigation system in compliance with drought/water conservation standards.
- ♦ To minimize water use, the following measures shall be incorporated into project design within the project area, where feasible: Use of drought tolerant plants, extensive use of mulch in landscaped areas, installation of drip irrigation systems where appropriate, and minimization of impervious area.
- ♦ All trees planted in turf areas shall receive turf boots to prevent damage from mowers and edgers, etc. Root barriers shall be required where trees are planted within five (5) feet of hardscape.
- ♦ Shrubs located at entries, front yards or major intersections shall have a minimum size of five (5) gallons with minor exceptions as approved by the Planning Director.
- ♦ Palm trees may be used only in special appropriate instances

as a part of the residential private yard or commercial parking lot landscape design. These trees are generally not reflective of rural Calabasas.

- ♦ Drought tolerant plant materials shall be utilized in all landscaping within the project area. Drought tolerant plants shall be defined as plants listed as Moderate, Low, and Very Low in the WUCOLS PROJECT listing of Water Use Classification of Landscape Species as published by the University of California Cooperative Extension. University of California Publications can be obtained from:

ANR Publications
 University of California
 6701 San Pablo Ave.
 Oakland, California 94608-1239
 (415) 642-2431

4. Appropriate Plant Materials

All plantings shall emphasize natives and avoid invasive species.

Trees:

Platanus acerifolia	London Plane Tree
Pistacia chinensis	Chinese Pistache
Pyrus calleryana "Aristocrat"	Aristocrat Pear
Tipuana tipu	Tipu Tree
Quercus agrifolia	Coast Live Oak
Ulmus parvifolia	Evergreen Elm
Eucalyptus Nicholii	Peppermint Gum
Populus Candidus	Poplar
Olea sp.	Olive

Tall Shrubs for setbacks, yards and parking lots:

These plants reach 3 to 10 feet in height at maturity and shall not be frequently sheared or pruned. The shrubs shall display flower and/or foliage color, and be resistant to prolonged periods of drought.

Acceptable species are:

Abelia grandiflora	Glossy Abelia
Escallonia fradesii	Escallonia
Elaeagnus pungens	Silverberry
Nerium oleander	Oleander
Photinia fraseri	Photinia
Plumbago auriculata	Cape Plumbago
Pittosporum tobira "Variegata"	Varegated Tobira
Xylosma congestum	Shiny Xylosma

Low shrubs, groundcovers in yards and parking lots:

These shall reach no more than 3 feet in height at maturity, without requiring frequent shearing and pruning. The shrubs shall display

flower and/or foliage color, and be resistant to prolonged periods of drought. Acceptable species are:

Cistus salvifolius	Prostrate Rockrose
Cotoneaster dammeri	Prostrate Cotoneaster
Euonymus fortunei "Colorata"	Purple Euonymous
Lantana montevidensis	Trailing Lantana
Mahonia repens	Prostrate Mahonia
Myoporum parvifolium	Prostrate Myoporum
Pittosporum tobira "Wheeler's"	Prostrate Tobira
Trachelospermum jasminoides	Star Jasmine

Hedge plantings, in lieu of a Streetwall:

These hedges can be pruned and sheared into a solid hedgerow of the specified height, without breaks or openings between individual shrubs. Acceptable species are:

Escallonia fradesii	Pink Escallonia
Ligustrum texanum	Wax Leaf Privet
Raphiolepis indica	India Hawthorn

Trash Enclosures and Streetwalls at Loading Areas:

These walls shall be planted with self-adhering vines no less than 10 feet on center and a minimum of 5 gallons in size. Acceptable species are:

Ficus pumila repens	Creeping Fig
Parthenocissus tricuspidata	Boston Ivy

SIGN STANDARDS

Signage can contribute positively to the overall-quality of the Master Plan area. Excellent signage serves as a communication tool and an art form. Businesses are encouraged to use signage in a manner that clearly expresses the spirit of their enterprises while enriching the streetscape views.

The Master Plan strives to provide sign standards that will reduce visual clutter and provide a cohesive streetscape that reflects a rural/rustic character.

1. Objectives

The following standards are meant to promote viable businesses by providing adequate identification, promoting high quality design and establishing appropriate scale and numbers. These standards are intended to supplement and add clarity to the Development Code requirements. All sign standards not listed in this Chapter are regulated by the City's Development Code. Also, all sign review and permitting shall be as regulated in the City's Development Code.

2. Allowed and Encouraged Signs

The following types of signs are established for the Master Plan area as they are found to be compatible with the rural/rustic theme of the Las Virgenes Gateway Master Plan:

Wall Signs - Flush mounted attached panel or individual letters.

Painted - Applied directly to the building in an appropriate location.

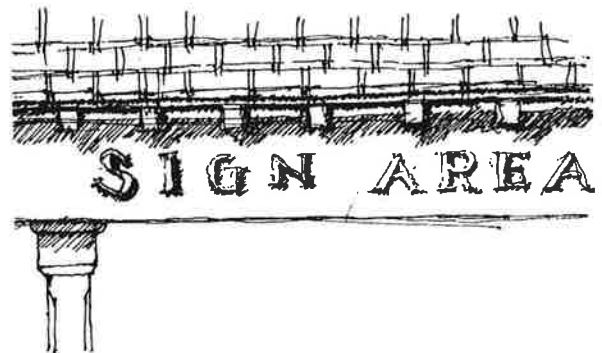
Projecting Signs or Marques - Attached to a building so that no part of the sign extends lower than 7-feet above the adjacent finished grade or 3-feet from the face of the building.

Figurative Signs - Any type of sign that uses a graphic or crafted symbol to advertise the occupant business. Examples are a shoe for a shoe store, a key for a lock smith, a book for a book store.

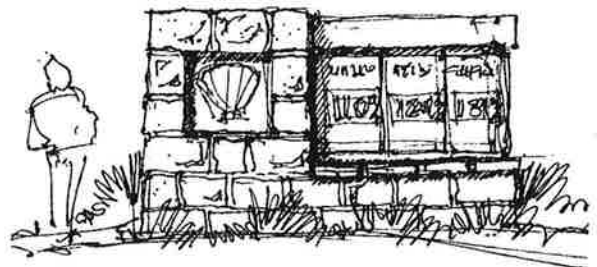
Awning and Canopy Signs - Lettering to be integrated into the front of the awning.

Applied Window Signs - Applied or painted onto the glass.

Monument Sign - Ground-mounted sign in a landscaped setting.



Building design should anticipate signage for tenants as an integral component of the building's aesthetics.



Signage can express the regional context and announce the quality of the business within.



3. Acceptable Sign Materials

The following materials are encouraged for use in signs to portray the rural/rustic theme of the Las Virgenes Gateway area:

Wood - Carved, sandblasted, etched, painted or stained.

Metal - Formed, etched, cast, engraved, or painted.

High Density Pre-formed Foam - New materials may be appropriate if properly designed in a manner consistent with the rural/rustic theme.

Custom Neon Tubing - May be sensitively integrated into allowed signs in the form of graphics or lettering.



Project signage does not need to identify the name of every store within - a technique that often leaves the sign cluttered and hard to read. Instead, the types of businesses and services can be announced succinctly and without clutter.

Unique Materials and Designs - Creative ideas not addressed but which are compatible with the rural/rustic theme may be considered.

Internally Illuminated - These signs, generally with a plastic face are discouraged but can be used in the Highway Triangle portion of the Master Plan subject to the restrictions in the Development Code and Scenic Highway Ordinance. The businesses in this area cater to visitors and motorists on the highway. These signs improve visibility for passing motorists.

4. Prohibited Signs

The following types of signs are prohibited in the Master Plan area as they add to visual clutter, are out of scale or do not contribute to the rural/rustic theme:

- Outdoor Advertising Signs or Billboards
- Roof Signs
- Rotating, Revolving or Flashing Signs
- Pole Signs
- Bench Signs
- Private signs in the public right-of-way.
- Internally illuminated signs except in the Highway Triangle area.

5. Specific Design Standards for Signs

Signs shall be designed to compliment the architectural character of the on-site structure and carry out the rural/rustic theme of the Las Virgenes Master Plan.

- ♦ Internally illuminated signs can only be placed in the Highway Triangle area of the Master Plan. These signs shall also contain elements of the rural/rustic theme.
- ♦ Monument signs shall be located in a landscaped area.
- ♦ Corporate signs and logos shall conform to the rural/rustic theme.
- ♦ Commercial centers with two or more tenants, office complexes and other similar facilities shall have a coordinated Sign Program in accordance with the City's Development Code.
- ♦ For buildings with one tenant the following signage is allowed:
 - no more than one sign per side of the building that faces a street, sidewalk or parking area. Each sign shall be limited to the square footage specified in the Sign Regulations of the Development Code; *and*

- a single monument sign of no more than 50 square feet and no higher than 6 feet when measured from the base of the monument to the top of the sign.
- ♦ For buildings with more than one tenant located in a building:
 - each tenant may have one individual building mounted sign for each occupancy frontage, of no more than 0.5 square feet per linear foot of store front, 80 square feet maximum total for all building mounted signs; *and*
 - window signs shall be limited to 25% of the glass area; *and*
 - a single monument sign of no more than 100 square feet and no higher than 8 feet when measured from the base of the monument to the top of the sign. The monument sign may list all businesses in the building.
- ♦ All signage shall be consistent with the Scenic Corridor Ordinance.

8 *Circulation*



CIRCULATION AND PARKING

GENERAL CIRCULATION ISSUES

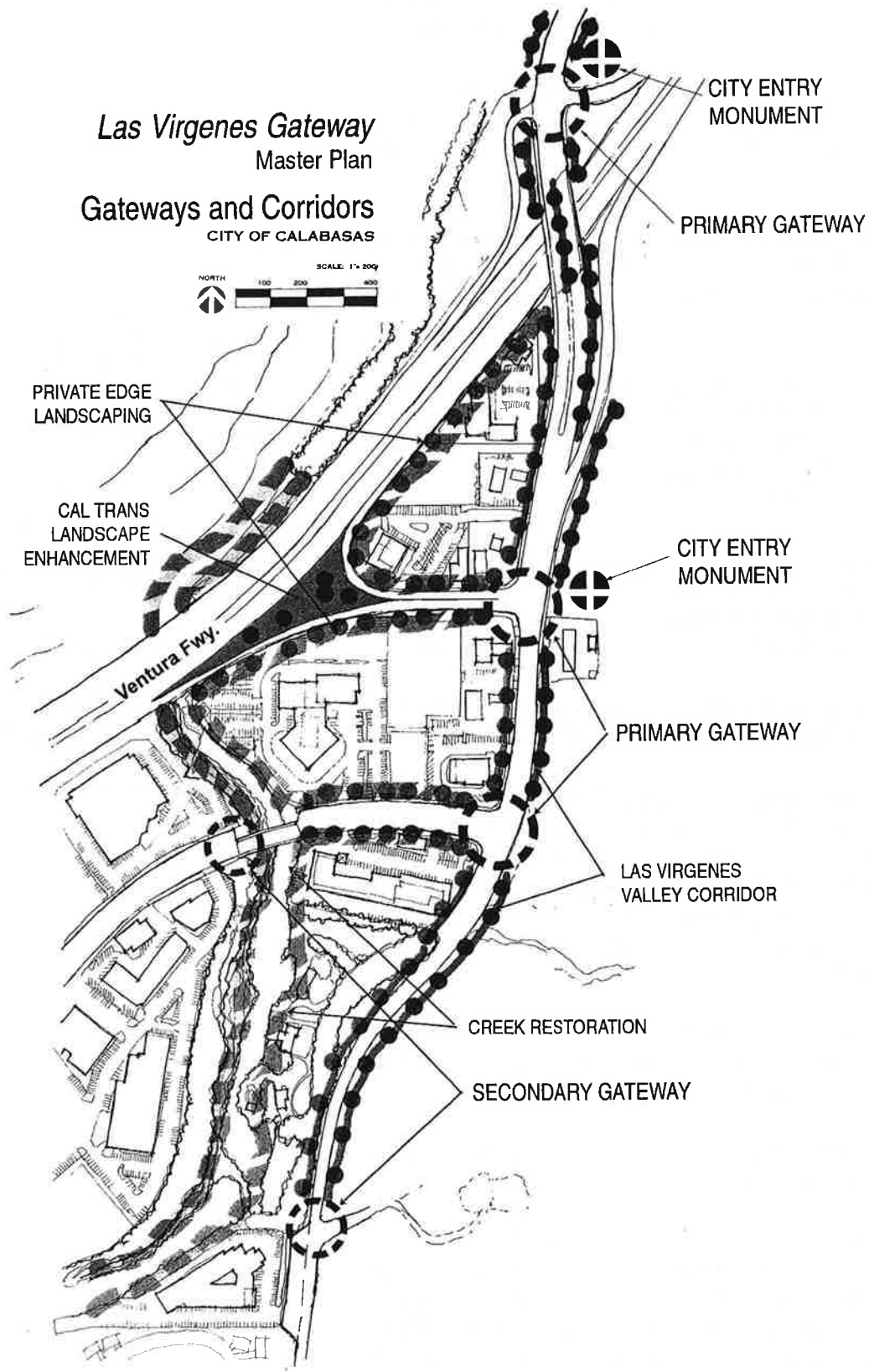
Las Virgenes Road is identified as a Critical Roadway Corridor in the City's General Plan. It is one of several roads that if not properly scaled to the local neighborhood and environmental conditions, could adversely effect Calabasas' quality of life. The segment of Las Virgenes Road immediately south of the Ventura Freeway reflects this concern. This segment is characterized by a very wide paved section, fast moving traffic, conflicting and uncontrolled driveway accesses and unregulated left turns. Las Virgenes Road north of Mureau Road has been identified as an street segment that has a physical capacity greater than is appropriate for its function in the community. This section of Las Virgenes Road serves as a local street for adjacent multi-family residential uses, many of which have driveway access to the street. This road segment is too wide to provide a safe and aesthetic travel corridor for the adjacent residential neighborhood.

The General Plan has an overall policy to promote roadway design that will optimize safe traffic flow within established road configurations by minimizing turning movements, uncontrolled access, on-street parking and promoting safe bicycle and pedestrian movement. To accomplish this policy, the General Plan outlines an Intersection Carrying Capacity Enhancement program for Las Virgenes Road.

The following list summarizes the circulation components of the General Plan that are to be carried out in the Master Plan. In addition to the General Plan circulation components, details for circulation improvement strategies are outlined in the *Las Virgenes Road Corridor Design Plan* prepared in January 1998 by RRM Design Group.

- ♦ Consider new signals, signal timing adjustments, re-striping, landscaping, signage, bicycle lanes, and turning movements.
- ♦ Reduce the number of driveways and left turn movements along Las Virgenes Road.
- ♦ Solutions may include appropriate placement of traffic signals, marked crosswalks, and pedestrian overpasses.
- ♦ South of the freeway, improvements for pedestrian and bicycle travel shall be provided south to Mulholland Highway.

Las Virgenes Gateway
Master Plan
Gateways and Corridors
CITY OF CALABASAS



- ♦ A right angle intersection should be created at the driveway intersection south of Agoura Road (Pontippidan property).
- ♦ The ultimate maximum roadway configuration shall be six through lanes between the Ventura Freeway and Agoura Road and four through lanes between Agoura Road and Lost Hills Road.
- ♦ A bike way is proposed for the length of Las Virgenes Road within the Master Plan area. The bike way plan has been designed in the *Las Virgenes Road Corridor Design Plan* as follows:

Lost Hills Road to Agoura Road -The bike way shall be a Class II-B on both sides of Las Virgenes Road. The width shall vary from 5 feet to 7 feet to avoid relocation of utility poles, where possible. The bike lane will share a right turn lane at Lost Hills Road intersection.

Agoura Road to Ventura Freeway - The bike way shall be a Class II-B on both sides of Las Virgenes Road. The width shall be 5 feet. A shared right turn lane will be necessary in the southbound lane at the Agoura Road intersection.

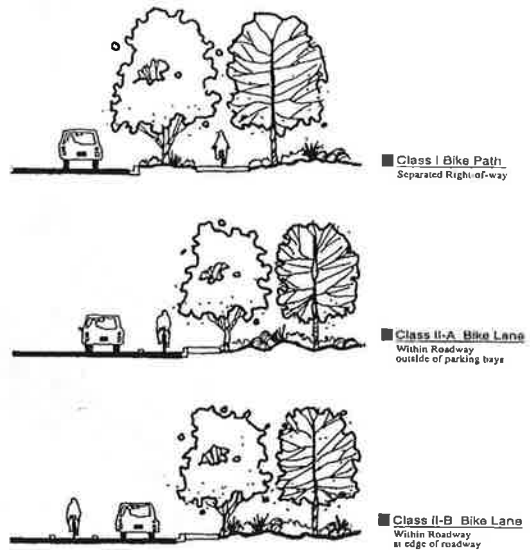
At the Ventura Freeway Overpass - Class II-B bike lanes, 5 feet wide shall be provided on both sides of the overpass.

Ventura Freeway to the Ventura County Line - A Class II-A bike lane, 5 feet wide, shall be provided on the east side of Las Virgenes Road. A Class II-B bike lane shall be provided on the west side from the freeway to Mureau Road. A Class I bike path shall be provided on the west side of Las Virgenes Road from Thousand Oaks Blvd. to the County line.

- ♦ Enhanced transit opportunities are called for in the General Plan. Transit enhancements have been designed in the *Las Virgenes Road Corridor Design Plan* as follows:

Each transit stop may have a covered structure, seating and bicycle racks or bicycle lockers.

Transit stops shall be provided along Las Virgenes Road within the Master Plan area on the east side of Las Virgenes Road at the southbound freeway on- and off-ramps, at the northwest corner of the freeway interchange, at the northeast corner of Mureau Road and Las Virgenes Road, at the northeast corner of Parkmor and Las Virgenes Road, and at the northwest and northeast corners of



Typical Bikeway Sections

the Las Virgenes Road and Thousand Oaks Blvd. intersection.

Trails for recreation and circulation opportunities are also addressed by the General Plan. The trail plan provides pedestrian and equestrian access to the hillsides and Malibu Creek areas.

MASTER PLAN OBJECTIVES

The following statements reflect the circulation/parking plan objectives of the Las Virgenes Gateway Master Plan:

1. Accommodate a large through-traffic volume as well as local residential, office, commercial and school traffic. Provide for this activity while calming traffic and creating a village center, especially at Agoura Road between the existing mixed use development and the proposed Neighborhood Commercial at the old dealership site.
2. Provide a landscaped median along the entire length of Las Virgenes Road in the Master Plan area.
3. Provide striping and signalization enhancements as recommended in the *Las Virgenes Road Corridor Design Plan*.
4. Provide a bicycle lane along the length of Las Virgenes Road.
5. Provide enhanced crosswalk paving at all intersections and at the Las Virgenes Creek bridge to enhance pedestrian circulation.
6. Provide access from Las Virgenes Road at Agoura Road to the east side properties. This road should be designed as a local street. A road extension through the east hillsides shall not occur.
7. Thousand Oaks Blvd. should not be extended to the west, beyond Las Virgenes Road.
8. Improve left-turn and U-turn movements from southbound Las Virgenes Road at the freeway ramps.
9. The main entrance for the neighborhood commercial center shall be on Agoura Road. Secondary or internal access to the commercial center is encouraged to be provided from properties on Las Virgenes Road.
10. Require reciprocal access and parking agreements for adjacent parcels, whenever feasible to reduce the number of drive-

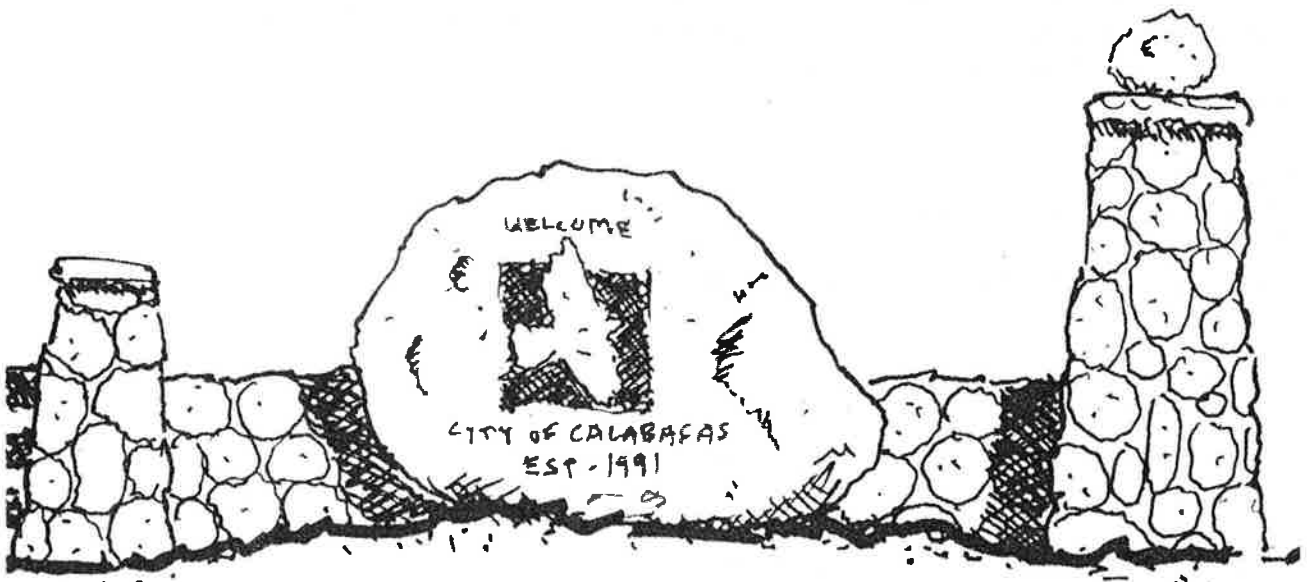
ways and to promote internal circulation.

11. Parking areas shall be designed to promote pedestrian circulation on the site and between adjacent sites and to allow generous landscaping.
12. A hiking and riding trail (the Las Virgenes Trail) shall be provided along Las Virgenes Creek from the Ventura Freeway south to the end of the planning area and beyond. A hiking and riding trail (Calabasas-Cold Creek Trail) shall also be provided from the Agoura Road/Las Virgenes Road intersection eastward to the Conservancy open space lands on the east hillsides. A pedestrian connection shall be provided between these two trails.
13. Provide a Park and Ride facility on Las Virgenes Road in the vicinity of the Rondell property.
14. Investigate access to the Steeplechase Trail.
15. The City shall pursue undergrounding of utility poles and lines.

It is the City's intent to require new developments to mitigate their traffic impacts, either through construction of new roadways or payment of mitigation fees. This system will distribute the costs of the street enhancements discussed above, based on the proportional share of traffic. The Circulation Plan Objectives shall be implemented through the City's Capital Improvement Program.

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9 *Public Improvements*



PUBLIC IMPROVEMENTS

The Las Virgenes Road corridor in the Master Plan area has been traditionally oriented to the highway traveler, very little attention has been given to the street scene as an entry point to western Calabasas. One of the most problematic issues is a lack of cohesiveness. This lack of community is observed in the confusing traffic movements, competing advertising signs, minimal landscaping and disjointed architectural styles.

The Master Plan's goal is to provide public improvements in addition to those outlined in the Circulation and Parking chapter, that will quiet this section of Las Virgenes Road. The objective can be accomplished through landscaping, controlled circulation, unified street furnishings, signage and encouragement of pedestrian and bike travel. The plan is to create a village environment with broad arching street trees, detailed fencing, light posts, banners, colorful landscaping and enhanced pedestrian movement.

STREETSCAPES

The *Las Virgenes Road Corridor Design Plan*, prepared in January 1998 by RRM Group, contains an extensive streetscape beatification plan for Las Virgenes Road. The Master Plan incorporates the theme from that plan with refinements to provide a comprehensive gateway streetscape plan.

1. Objectives

The streetscape plan has the following objectives:

1. Promote development of high quality street scenes that reinforce the rural image.
2. Ease tensions between pedestrian and vehicular traffic.
3. Employ landscape treatments to screen negative views and enhance or frame positive views.
4. Identify primary gateways and provide a sense of hierarchy and continuity with secondary gateways.
5. Provide a unified rural theme area along Las Virgenes Road from the Ventura Freeway south to Lost Hills Road and along Agoura Road from its intersection with Las Virgenes Road to the bridge at Calabasas Creek.
6. Place tree plantings to preserve public view corridors from Las

The Master Plan's goal is to provide public improvements in addition to those outlined in the Circulation and Parking chapter, that will quiet this section of Las Virgenes Road. The objective can be accomplished through landscaping, controlled circulation, unified street furnishings, signage and encouragement of pedestrian and bike travel.



Virgenes Road.

2. Design Elements

The streetscape design elements include the following:

Landscaped medians with river rock centers and textured concrete detail on noses.

Special paving at pedestrian street crossings.

New sidewalk paving with decorative tile or brick/paving treatment.

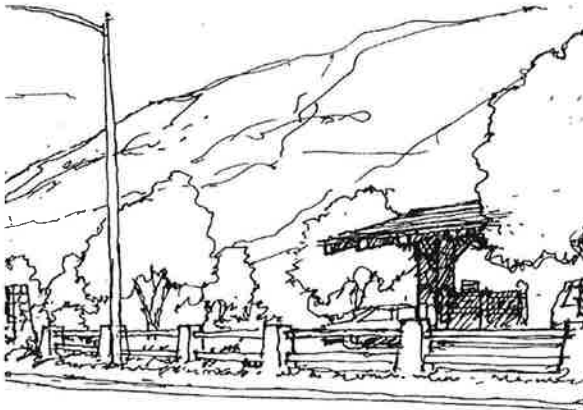
Street trees with tree grates and special pavement surrounds.

Decorative lamp posts with custom banners.

Special fencing at back of sidewalk.

Site furnishings: benches, trash receptacles, planters, bike racks.

Under grounding utility lines.



3. Street Plantings

Las Virgenes Road - The median tree for the segment of Las Virgenes Road from Lost Hills Road to Mureau Road shall be *Populus candicans* (Balm of Gilead) at a spacing of no less than thirty feet on center. Trees shall be planted in a straight row and centered in the median. Ground cover and paving in the medians shall be predominantly river rock "cultured stone" with pockets of prostrate and accent shrubs planted in drifts and groupings. Prostrate shrubs shall be Lantana, Rockrose, and Cotoneaster. Accent plants shall be Fortnight Lily, Fountain Grass and Compact Flax.

Sidewalks shall be planted with *Platanus acerifolia* / London Plane Tree at a spacing of no less than forty feet on center. Trees shall be planted in four-foot square tree wells behind the curb face. For the segment of Las Virgenes Road from Mureau Road north to Thousand Oaks Blvd, the median tree shall continue to be the existing London Plane Tree.

For the segment of Las Virgenes Road, north of Mureau Road, sidewalk trees shall be *Populus candicans*. Medians in this segment shall be developed with river rock and prostrate plantings similar to the southern segment of Las Virgenes Road.

Agoura Road - Sidewalks shall be planted with *Schinus molle* (California Pepper Tree) at a spacing of no less than forty feet on center in tree wells flanking the street.

Highway 101 Ramps - Planters outside of the Caltrans Right of Way shall be planted with American Sweetgums (*Liquidambar styraciflua*) no less than 30 feet on center in parkway strips flanking the street. Planting within the Right of Way shall be of species approved Caltrans.

Private Circulation Alleys - Alleys shall be detailed as minor streets, with frequent curb breaks for access to parking and loading. Parkway strips behind the curb are to be planted with trees at 25 to 30 feet on center plus turf or groundcover. The planting strip behind the sidewalk shall be planted with shrubs, with or without ground plantings. Wall vines may be planted on adjacent building walls and streetwalls, and shall be planted on streetwalls over 42 inches in height.

GATEWAYS

City entries are primary points of arrival, or *gateways*. These entries occur at the Highway 101 southbound and northbound off-ramps at Las Virgenes Road. These entrances to western Calabasas should be distinctive and announce a point of arrival to this

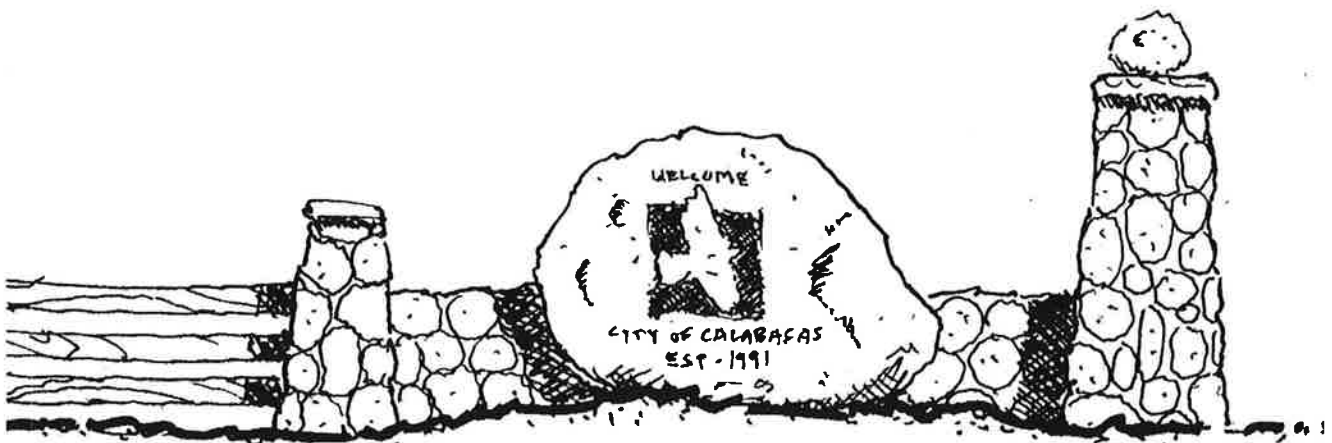
rural valley corridor. The design should create a memorable landmark incorporating the City logo and a welcome statement. Monument design and landscape treatments should be bold to relate to vehicular traffic. Paving materials, trees, and rural fencing should be laid out to emphasize this as the western City entry.

These stone monuments should be placed at the northeast corner of the Las Virgenes Road intersection with the northbound freeway off-ramp and at the northeast corner of the Las Virgenes Road intersection with the southbound freeway off-ramp. Placement of these monuments should become a part of the site planning and development review for new or renovated development on these properties.

Secondary gateways occur at various locations where visitors and residents traverse Las Virgenes Road and Agoura Road. These internal gateways occur in locations that link the Las Virgenes corridor with surrounding neighborhoods. These secondary gateways are as follows:

- The intersection of Las Virgenes Road and Agoura Road.
- The Agoura Road Bridge over Malibu Creek.
- The intersection of Las Virgenes Road and Thousand Oaks Boulevard.
- Southerly end of the Master Plan area on Las Virgenes Road.

The secondary gateways should enhance the pedestrian experience and slow down traffic. Pedestrian-scale plantings, detailed paving, special street lights, and benches should be incorporated. These areas should be detailed with rural theme elements that echo the primary gateway but do not compete with it.



Conceptual study of gateway monumentation at key entry points into the City.

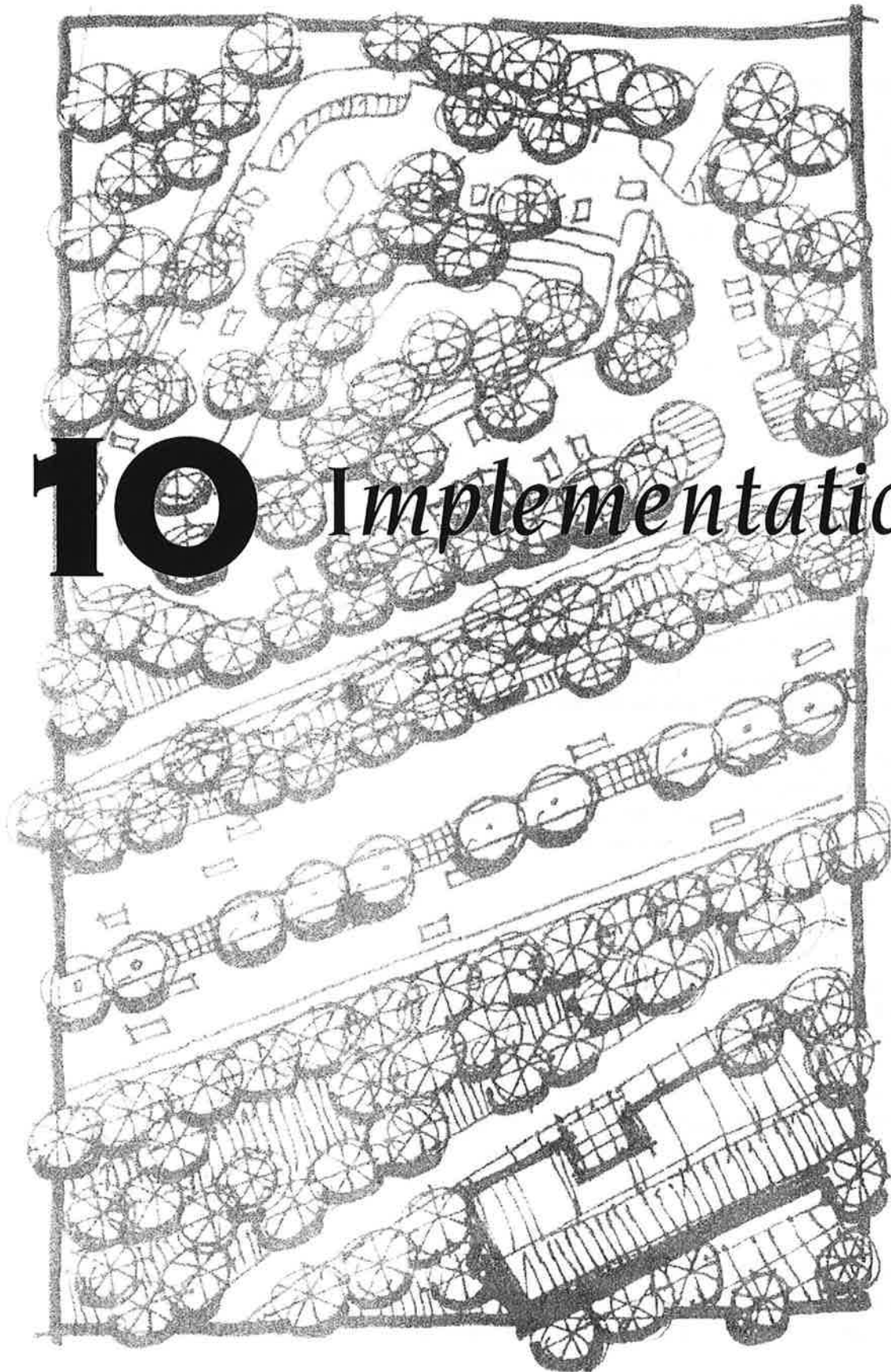
FREEWAY ENHANCEMENTS

Landscape treatments along the Highway 101 freeway and off-ramps can play an integral part in the success of the design treatment. Landscape treatments along these approaches should be simple with a strong sense of greenery contrasting with the appearance of the freeway and off-ramps. This provides a pleasant foreground announcing the gateways. Coordination with Caltrans will be required for this planting.

OVERHEAD UTILITY LINES

All new construction or restoration of existing buildings will be required to place all new utility lines underground. Also, existing utility lines should be placed underground whenever feasible. If existing lines cannot be placed underground, consideration should be given to relocation to an area where they are less visible. The City shall also investigate the feasibility of undergrounding utility lines as part of any streetscape construction project.

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10 *Implementation*

IMPLEMENTATION

MASTER PLAN ADMINISTRATION

The Las Virgenes Gateway Master Plan contains the tools to establish and maintain the vision of a memorable gateway and the rural setting. Goals, objectives and standards are provided to direct renovations and new development. This chapter provides information relative to achieving the Plan. As Plan implementation is dependent on both private and public development, there must be a coordination of efforts. To assure Plan implementation, the following requirements are established:

- ♦ The City shall adopt the Las Virgenes Gateway Master Plan and associated General Plan and Zoning amendments so that the Master Plan goals, objectives and standards can be used in the review of development applications. The adoption of this Master Plan also concurrently adopts the associated General Plan Amendments and establishes the Las Virgenes Gateway Overlay Zone.
- ♦ The City shall adopt a plan for carrying out public improvements as described in the *Las Virgenes Road Corridor Design Plan* and this Master Plan, including the components listed below. The public improvements shall be coordinated and prioritized through the City's Capital Improvement Program.

- ♦ Streetscape Improvements
- ♦ Entry Gateway Monumentation
- ♦ Intersection/Pedestrian Crossing Decorative Paving
- ♦ Roadway Improvements (turn lanes and medians)
- ♦ Bridge Widening
- ♦ Planted Medians
- ♦ Underground Utilities

- ♦ The City shall investigate available grants for creek reclamation and trail construction. The City shall apply for those that are determined to be appropriate.
- ♦ The City shall establish an incentive program for new development and renovations in the Master Plan area. The incentives can include, lower processing fees, fast-track permit processing, grants and low interest loans.

MASTER PLAN AMENDMENTS

From time to time it may be necessary to amend the Master Plan to respond to specific environmental or economic changes or to ac-

commodate newly identified opportunities. The Plan should be amended by resolution. Whenever feasible, development permits should be considered at the same time as the Plan amendment request.

REGULATORY MEASURES

The City can implement the Master Plan through land use and development controls. To implement the regulatory measures in the Master Plan the following steps must be taken:

1. General Plan Text Amendments - The General Plan Amendments outlined in the Master Plan and in Appendix D will occur at adoption of the Master Plan.
2. General Plan Land Use Map Amendments - The following properties will have General Plan Land Use Designation amendments adopted concurrently with adoption of the Master Plan::

Rondell (APN 2069-031-014 and 015) Change from Urban Hillside to Business-Retail.

Pazar (APN 2069-020-025 and 026) Change from Business-Retail to Residential-Multiple Family

Malibu Commercial Center Parcel (APN 2064-020-008) Change the land use designation from Single Family Residential to Business-Retail to correct an existing General Plan mapping error.

3. Baldwin/Village (APN 2069-078-009 and 011) Land use designation boundaries may need to be changed to implement the Master Plan, however the land use designations should not change.
4. Development Code Amendment - Adopt the Las Virgenes Gateway Overlay Zone.
5. Zone Map Amendments - The following properties shall have Zoning Designation changes:

Adopt the Las Virgenes Gateway Overlay Zone for all properties in the Plan boundary.

Rondell (APN 20-031-014 and 015) Change from Hillside/Mountainous to Commercial-Retail.

Pazar (APN 2069-020-025 and 026) Change from Commer-

cial-Retail to Residential-Multiple Family Planned Development.

6. Baldwin/Village (APN 2069-078-009 and 011) Zone designation boundaries may need to be changed to implement the Master Plan, however the existing zone designations should not change.
7. Architectural and Landscape Design Standards - Adopt the Master Plan Design Standards in Chapter 7 by resolution.
8. Sign Standards - Adopt the Master Plan standards by ordinance.

ANNEXATION

A large portion of the property located along the Las Virgenes corridor north of the Ventura Freeway is not within the City's jurisdiction. These lands are in Los Angeles County. This split jurisdiction in the middle of the Master Plan area affects the City's ability to provide land use and design controls consistent with the Master Plan. It is recommended that these lands be considered for annexation to the City. The City should initiate proceedings with the Local Agency Formation Commission. Possible land uses for these properties include low density residential, a public school and recreation fields.

Until such time as these lands are annexed, the City should use the land use and design recommendations in the Master Plan to provide input to the County on any development proposals on these lands.



Appendices

Board
Council
Planning
Administration
Community
Development
Finance
Health
Housing
Public Works
Police
Public Safety
Social Services
Transportation
Utilities

Appendix A

Community Workshop and Public Hearing Input

